PUBLIC NOTICE

REQUEST FOR PERMISSION TO ALTER A U.S. ARMY CORPS OF ENGINEERS PROJECT UNDER SECTION 408

TITLE: City of Lewes Board of Public Works – Installation of New Water Main Utility Line beneath the Inland Waterway from Rehoboth Bay to Delaware Bay Federal Navigation Channel (also known as the "Lewes and Rehoboth Canal" and "L&R Canal") in the City of Lewes, Sussex County, Delaware

PUBLIC NOTICE IDENTIFICATION NUMBER: 408-NAP-2024-0017

PUBLIC NOTICE COMMENT PERIOD:

Begins: 25 September 2024

Expires: 25 October 2024

Interested parties are hereby notified that an application has been received for a Department of the Army Section 408 permission for certain work at or near a federal project of the United States, as described below and shown on attached figures. Written comments are being solicited from anyone having an interest in the requested alteration. Comments will become part of the U.S. Army Corps of Engineers' (USACE's) administrative record and will be considered in determining whether to approve the request. Comments supporting, opposing, or identifying concerns that should be considered by the USACE in its decision process are all welcome.

This public notice is not a paid advertisement and is for public information only. Issuance of this notice does not imply USACE endorsement of the project as described.

- **1. REQUESTOR:** In compliance with 33 USC 408 (Section 14 of the Rivers and Harbors Act of 1899; hereinafter Section 408), the City of Lewes Board of Public Works has requested permission to install, via the Horizontal Directional Drilling (HDD) method, one (1) new water main utility line beneath the Inland Waterway from Rehoboth Bay to Delaware Bay Federal Navigation Channel (also known as the "Lewes and Rehoboth Canal" and "L&R Canal").
- **2. LOCATION:** The proposed project will cross the Inland Waterway from Rehoboth Bay to Delaware Bay Federal Navigation Channel in the City of Lewes, Sussex County, Delaware, at approximate coordinates 38.773614, -75.133742.
- 3. LOCATION MAP(S)/DRAWING(S): Please see attached Project Plan Sheets 1-19.
- **4. REQUESTER'S PROPOSED ACTION:** The proposed action entails the installation, via the HDD method between upland entry and exit pits, of approximately 2,000.0-linear feet of

18.0-inch diameter high-density polyethylene (HDPE) water main piping, with an approximately 265.0-linear foot portion crossing beneath the canal. The water main piping will cross beneath the Inland Waterway from Rehoboth Bay to Delaware Bay Federal Navigation Channel portion of the canal at a depth of 25.0-35.0-feet beneath the existing canal bottom.

- **5. REGULATORY AUTHORITY:** This request will be reviewed according to the provisions of Section 14 of the Rivers and Harbors Act of 1899 (33 U.S.C. 408). A requestor has the responsibility to acquire all other permissions or authorizations required by federal, state, and local laws or regulations, including any required permits from the USACE Regulatory Program under Section 10 of the Rivers and Harbors Act of 1899 (33 USC 403), Section 404 of the Clean Water Act (33 USC Section 1344) and/or Section 103 of the Marine Protection, Research and Sanctuaries Act of 1972 (33 USC 1413). Any Section 10/404/103 permit decision associated with the proposed alteration is separate from and will not be included in the Section 408 permission decision. An approval under Section 408 does not grant any property rights or exclusive privileges nor does it authorize any injury to the property or rights of others.
- **6. ENVIRONMENTAL COMPLIANCE:** A decision on a Section 408 request is a federal action, and therefore subject to the National Environmental Policy Act (NEPA) and other environmental compliance requirements. While ensuring compliance is the responsibility of USACE, the requester is providing all information that the Philadelphia District identifies as necessary to satisfy all applicable federal laws, executive orders, regulations, policies, and ordinances. Based on information provided by the applicant to date, current Corps regulations governing NEPA implementation, and/or the contents of existing NEPA documentation if available, it is likely that the proposed action will be determined to be categorically excluded from the need to prepare an Environmental Assessment (EA) or Environmental Impact Statement (EIS). This determination will be finalized following completion of agency coordination and prior to issuance of the Section 408 Permission Decision.
- **7. EVALUATION:** As part of its evaluation, USACE will first make a determination that the submittal from the requestor is complete. The Philadelphia District is working closely with the requestor to ensure that all required technical plans, maps, drawings, and specifications are provided and are complete. Once the package is complete, a District-led review will be conducted to determine, in accordance with Engineering Circular (EC) 1165-2-216, whether the proposed alteration will impair the usefulness of the USACE Project or be injurious to the public interest, as follows:
 - A. *Impair the Usefulness of the Project Determination*. The Philadelphia District's Section 408 review team will determine if the proposed alteration will limit the ability of the federally authorized project to function as authorized, or will compromise or change any authorized project conditions, purposes or outputs.
 - B. *Injurious to the Public Interest Determination*. Proposed alterations will be reviewed to determine the probable impacts, including cumulative impacts, on the public interest. Evaluation of the probable impacts that the proposed alteration to the USACE project may have on the public interest requires a careful weighing of all

those factors that are relevant in each particular case. Factors that may be relevant to the public interest depend upon the type of USACE project being altered and may include, but are not limited to, such things as conservation, economic development, historic properties, cultural resources, environmental impacts, water supply, water quality, flood hazards, floodplains, residual risk, induced damages, navigation, shore erosion or accretion, and recreation. The decision whether to approve an alteration will be determined by the consideration of whether benefits are commensurate with risks. If the potential detriments are found to outweigh the potential benefits, then it may be determined that the proposed alteration is injurious to the public interest.

- **8. SOLICITATION OF COMMENTS:** The USACE is soliciting comments from the public; federal, state, and local agencies and officials; Indian Tribes; and other interested parties in order to consider and evaluate the impacts of the proposed activity. Any comments received will be considered by USACE to determine whether to issue, modify, condition, or deny a permission for this proposal. To make this decision, comments are used to assess impacts on endangered species, historic properties, water quality, general environmental effects, and other public interest factors listed above. Comments are considered in making a final determination whether the proposed action will be categorically excluded from the need to prepare further NEPA documentation. Comments are also used to determine the need for a public hearing and to determine the overall public interest of the proposed activity.
- A. It should be noted that materials submitted as part of the Section 408 request become part of the public record and are thus available to the general public under the procedures of the Freedom of Information Act (FOIA). Individuals may submit a written request to the Philadelphia District Corps of Engineers, Office of Counsel to obtain copies of said materials under the FOIA.
- B. It is presumed that all parties viewing this notice will wish to respond to this public notice; therefore, a lack of response will be interpreted as meaning that there is no objection to the project as described.
- **9. COMMENT SUBMISSION AND ADDITIONAL INFORMATION:** Written comments on the described work should reference the USACE Public Notice Identification Number found on the first page of this notice. Comments must reach this office no later than the stated expiration date of the Public Notice to become part of the record and be considered in the decision. Comments or requests for additional information should be emailed or mailed to the following address:

Email: Bishel.Baby@usace.army.mil

Mailing Address:

U.S. Army Corps of Engineers

Philadelphia District

Attn: Bishel B. Baby

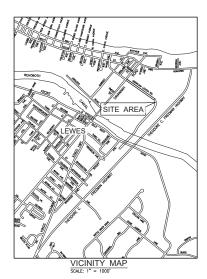
1650 Arch Street, 7th Floor

Philadelphia, PA 19103-2004

CITY OF LEWES SUSSEX COUNTY, DELAWARE



GMB FILE NO. 220183



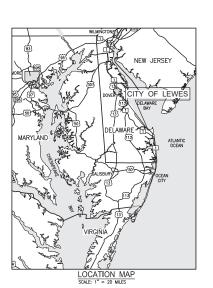
GENERAL NOTES

- . DRAWINGS ARE BASED ON FIELD SURVEYS BY GEORGE, MILES AND BUHR, LLC. PERFORMED DURING SEPTEMBER AND OCTOBER OF 2023. A PORTION OF THE INFORMATION RELATED TO THE REDAMS OF THE OLD PRAILINGS RIGHGS -1928 MAS INSEAN FROM PURS FOR RELOID CONTINENT TOTO/7076/2, DATED MAY 18, 2021. A PORTION OF THE INFORMATION RELATED TO EXISTING WAITER AND SERVER MARKS HAST FAREN FROM MYROUS SESSION AND AS-BUILT DRAWINGS. HOSTOPHICAL CONTROL IS NADRS VERTICAL CONTROL IS NAVORR
- NOBES, VEHICLE, CONTINCE, IS NOVOBES.

 2. PROPERTY AND ROHL-OF-WAY LIBES INDICATED ON THE DRAWING FOR THE KENNEDY PARCEL
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 AMPRING, LICE PHITLING PROLINGERY SURVEY, LANGS VI/F OF ROBERT J. RUNNEDY IN, INSTITUTE
 AMERICAN LEGION ROOA AREA ARE BASED ON RECORRED SUBBINISON PLATS PREPARED BY CEORRE,
 ALIES AND BURNE, LICE AND ATAINTIC SURVEYING & AMPIRIS, LLC. DETITIED "AMERICAN LEGION
 ROOA SUBDIVISION PLAN, WASTERNITER TREATMENT FACILITY SUBDIVISION PLATS PREPARED BY CEORRE,
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 AND SUBDIVISION PLANS WASTERNITE TREATMENT FACILITY SUBDIVISION PLATS FALLING AND AND ATE AND ATE
- BATHYMETRIC SURVEY WITHIN THE CANAL WAS PERFORMED BY PLITKO, LLC, OCEAN VIEW, DELAWARE ON MAY 19, 2023.
- THE FEDERAL CHANNEL LINES SHOWN FOR THE CANAL WERE TRACED FROM DRAWING E-LR-04
 DATED MARCH 15 & 16, 2022. PREPARED BY THE U.S. ARMY CORPS OF ENGINEERS, PHILADELPHIA
 DISTRICT, ENTITLED "NILAND WATERWAY, REHOBOTH BAY TO DELAWARE BAY, LEWES AND REHOBOTH
 CANAL, DELAWARE".
- FEDERAL WETLAND "404" LINES ALONG AMERICAN LEGION ROAD AND THE BIKE TRAIL / OLD RAUROAD BED WERE DETERMINED BY EVELYN MAURMEYER OF COASTAL & ESTUARINE RESEARCH, INC. OF LEWES, DELBHAMER ON APRIL 18, 2024.
- 6. VERTICAL DATUM CONVERSIONS TO MLW AND MILW DATUMS WAS TAKEN FROM THE DATA SHEET FOR TIDAL BENCH MARK I.D. \$6557380. MEAN TIDE RANGE IS 4.4', AS PUBLISHED FOR ROOSEVELT INLET.
- A PORTION OF THE PROJECT SITE IS WITHIN THE 100 YEAR FLOOD ZONE AE (ELEVATIONS 6 AND 7), AS SHOWN ON FEMA FIRM MAP NO. 10005C0194K, DATED MARCH 16, 2015.
- EXISTING UNDERGROUND UTILITY LOCATIONS SHOWN ARE APPROXIMATE ONLY. THE CONTRACTOR SHALL VERIFY LOCATION, SIZE AND INVERT OF EXISTING UTILITIES BEFORE BEGINNING ANY EXCAVATION OR PIPE LAYING TO GIVE TIMELY ADVANCE NOTICE TO ENGINEERS OF ANY CONFLICT BETWEEN EXISTING AND NEW WORK.
- BEFORE ANY EXCAVATION IS PERFORMED, CONTRACTOR SHALL CALL "MISS UTILITY" (1-800-282-8555) AND CITY OF LEWES BOARD OF PUBLIC WORKS (302-645-6228) 48 HOURS PRIOR TO EXCAVATION TO HAVE MUBERGROUND UTILITIES LOCATED AND MARKED. THE CONTRACTOR SHALL NOTITY ALL UTILITY ALL UTI
- 10. KEEP EXISTING UTILITIES IN SERVICE TO THE EXTENT POSSIBLE.

- 11. THE FAILURE TO SHOW ON THE CONTRACT DOCUMENTS ANY EXISTING UTILITIES SHALL NOT RELIEVE THE CONTRACTOR OF HIS OR HER RESPONSIBILITIES OF DETERMINING THE LOCATION OF THESE UTILITIES. INTERQUIPTION OF SERVICE SHALL BE REPARED BY THE CONTRACTOR ACCORDING TO THE CITY OR UTILITY COMPANY SPECIFICATIONS. THE CITY SHALL BE NOTIFIED OF ANY DAMAGE TO ANY UTILITIES.
- 12. CONTRACTOR SHALL PROVIDE ALL NECESSARY STAKEOUT OF LINE AND GRADE FOR ALL
- 13. CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF ALL PERMITS SECURED WHICH TAKE PRECEDENCE OVER SPECIFICATIONS.
- 14. PROVIDE A MINIMUM HORIZONTAL SEPARATION BETWEEN WATER AND SEWER MAINS OF $10^{\circ}-0^{\circ}$. HOUSE OF MEMBERS BROKEN, ALL PRIVATE OF THE SERVICE OF THE SERVICE
- CONTRACTOR SHALL ADJUST TO FINISH GRADE AS REQUIRED, ANY VALVE BOXES, MANHOLES, ETC., PRIOR TO RESURFACING THE STREET.
- THE CONTRACTOR SHALL REPAIR OR REPLACE IN-KIND ANY EXISTING FEATURES DAMAGES OR DESTROYED DURING CONSTRUCTION.
- ANY DISTURBED AREAS OUTSIDE THE RIGHT-OF-WAY SHALL BE RESTORED TO THEIR ORIGINAL CONDITION IMMEDIATELY.
- 18. THE CONTRACTOR SHALL REMOVE AND IMMEDIATELY RELOCATE/RESET/RECONSTRUCT ALL FENCES, SIGNS, PLANTERS, HOUSE LAMPS, STREET SIGNS, DRIVEWAYS, PARKING LOTS, AND ANY OTHER TYPES OF OBSTRUCTIONS WHETHER NATURAL OR MANMADE, ETC. WHENEVER REQUIRED AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL MAINTAIN ACCESS TO EXISTING DRIVEWAYS, PARKING LOTS AND OTHER ENTRANCES AT ALL TIMES.
- ALL BACKFILLED AND DISTURBED AREAS ARE TO BE SEEDED AND MULCHED WITH 4" OF TOPSOIL TO BE PLACED IN FILL AREAS.
- 21. ALL CONSTRUCTION SHALL BE DONE IN COMPLIANCE WITH THE OCCUPATIONAL SAFETY AND HEALTH ACT, LATEST EDITION, AND ALL RULES AND REGULATIONS THERETO.
- 22. TRAFFIC CONTROL SIONS SHALL REMAIN FUNCTIONAL AND VISIBLE TO THE APPROPRIATE LANES OF TRAFFIC AT ALL TIMES, THE COST OF TRAFFIC CONTROL SHALL BE INCLUDED IN THE PRICES BID. BLOCKFILL TREVIOURS AND INSTALL REMOUNDS VISIBLESS AS DESCRIBED ON SHEETS GIT AND CALS, AMERICAN LEGION ROAD AND THE PORTION OF GILLS NECK ROAD IN THIS PROJECT AREA ARE MAINTAINED BY THE CITY OF LURISON.

23. MINIMUM COVER FOR ALL WATER MAINS SHALL BE 3'-6"



DRAWING LIST

COVER SHEET LEGEND AND DELDOT UTILITY PLAN NOTES G1.1

C1.1 THRU C1.5 SITE PLANS

C2.1 THRU C2.3 EROSION AND SEDIMENT CONTROL NOTES AND DETAILS

C3.1 THRU C3.3 WATER MAIN PROFILES

C4.3 TRAIL AND ROAD RESTORATION DETAILS

TRAFFIC CONTROL PLAN

RE-ALIGNMENT OF G-L TRAIL AT GILLS NECK ROAD (PREPARED BY DELDOT)

GENERAL NOTES FOR WORK WITHIN DELAWARE'S FORMER RAILROAD PARCEL

- 1. CONTRACTOR SHALL FOLLOW ALL REQUIREMENTS OF DELDOT'S CURRENT CONSTRUCTION SPECIFICATIONS.
- 2. PIPELINE AND CROSSING TO BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH LAST APPROVED "AMERICAN RAILWAY ENGINEERING AND MAINTENANCE OF WAY ASSOCIATION SPECIFICATIONS FOR PIPELINES CONVEYING FLAMMABLE AND NON-FLAMMABLE SUBSTANCES".



AUGUST 2024

COVER SHEET

WATER MAIN CROSSING OF THE LEWES & REHOBOTH CANAL

CITY OF LEWES COUNTY, DELAWARE

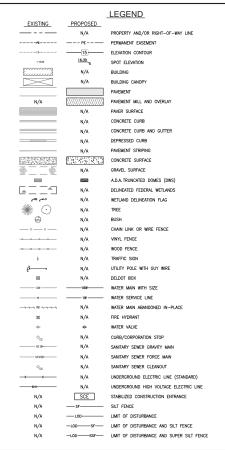
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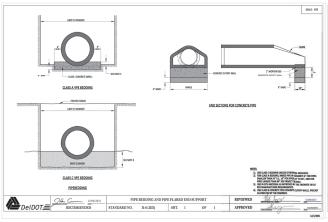
DESIGN / DEVELOPMENT PLANS

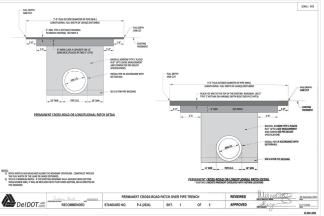
SOME : AS NOTED
DESIGN BY : JWK
DRAWN BY : JWK
CHECKED BY : VAL,COD SHEET NO G1.0 MB FILE : 220183

- PLANS ARE REVIEWED FOR GENERAL CONFORMITY. DELDOT IS NOT RESPONSIBLE FOR ERRORS OR OMISSIONS WITHIN THE PLAN SET. THE UTILITY OWNER IS RESPONSIBLE TO ENSURE ACCURACY OF PLANS AND
- MANHOLE TOP SECTIONS WILL BE OFF SET CONE STYLE, ANY MANHOLE LIDS THAT ARE DETERMINED BY DELDOT TO NOT BE IN THE CENTER OF THE LANE SHALL BE ADJUSTED PRIOR TO FINAL PAVEMENT PLACEMENT.
- MANHOLE FRAME AND LIDS SHALL BE INITIALLY SET ½" LOW AND ADJUSTED TO FINISH GRADE WITH CONCRETE COLLAR AFTER FINAL PAVEMENT HAS BEEN PLACED.
- CONCRETE COLLARS SHALL BE POURED AROUND MANHOLE FRAME AND LIDS/VALVE BOXES TO FINISH GRADE
 USING CLASS "A" CONCRETE.
- ALL BACKFILL MATERIAL IN EXISTING/PROPOSED ROADWAY SHALL CONFORM TO TYPE "C"BORROW. ALL BORROW BACKFILL SHALL BE COMPACTED TO 95% USING AASHTO T99 STANDARD FOR TESTING.
- 6. GABC PLACED SHALL BE COMPACTED TO 98%.
- COMPACTION TESTING SHALL BE PERFORMED EVERY 100' AND TESTING SHALL BE TAKEN ON EACH LIFT OF MATERIAL PLACED.
- TAR CHIP/HOT MIXES ROADS: TRAVEL WAY PAVEMENT DISTURBED SHALL BE RESTORED AT THE END OF THE DAY PRIOR TO REOPENING TO TRAFFIC. HOT MIX SHALL BE PLACE PER TEMP PATCHING DETAIL 6"GABC AND 2"TYPE" "O"HOT MIX.
- TAR CHIP/HOT MIXES SHOULDERS: SHOULDERS DISTURBED MAY BE LEFT IN CABC TO FINISH GRADE OVERNIGHT BUT SHALL BE CLOSED USING APPROPRIATE SIGNING AND DRUMS. TEMP PAVEMENT SHALL BE PLACED FOR SHOULDERS AT THE END OF EACH WORK WEEK.
- 10. IF THE REMAINING PORTION OF HOTMIX BETWEEN THE PIPE TRENCH EXCAVATION AND EDGE OF PAVEMENT IS LESS THAN 3' THE REMAINING SECTION SHALL BE REMOVED AND REPAYED AS PART OF THE FULL DEPTH PAVING RESTORATION.
- 11. ALL AREAS DISTURBED OUTSIDE OF THE PAVEMENT SHALL BE GRADED EACH DAY TO ENSURE POSITIVE DRAINAGE AND SHALL BE PERMANENTLY RESTORED AT THE END OF EACH WEEK.
- ALL TEMPORARY HOT MIX SHALL BE PLACED TO PROVIDE A SMOOTH RIDABLE SURFACE TO DELDOT STANDARDS.
- 13. A SAFETY EDGE IS REQUIRED ON ALL HOT MIX PLACED.
- 14. ANY STRIPING DISTURBED SHALL BE REPLACED AT THE END OF THE DAY PRIOR TO OPENING TO TRAFFIC.
- 15. PROOF ROLL OF GABC SHALL BE PERFORMED USING A LOADED 10 WHEELER PRIOR TO PLACEMENT OF HOT
- 16. ALL MATERIAS AND WORKMANCHE WITHIN THE STATE RAY CHALL BE COMPLETED IN ACCORDANCE WITH CURRENT STATE OF DELAWAGE DEPARTMENT OF TRANSPORTATIONS ASTROMADE SPECIATION FOR ROAD AND BRIDGE CONSTRUCTION, SUPPLIENTIAL SPECIA
- 17. THERE IS A ONE YEAR WARRANTY ON ALL EARTH WORK AND CONCRETE. A THREE YEAR WARRANTY ON ALL HOT MIX INCLUDING SUBBASE/SUBGRADE ISSUES WITHIN THE PAVEMENT AREAS. WARRANTY DOES NOT START UNTIL ALL WORK IS COMPLETED AND A STAND OF GRASS HAS BEEN ESTABLISHED TO DELDOT STANDARDS AND A ACCEPTANCE LETTER HAS BEEN ISSUED.
- 18. ALL DISTURBED AREAS WITHIN THE STATE RIGHT-OF-WAY, BUT NOT IN THE PAVEMENT, SHALL BE TOP-SOLED (6" MINIMUM), FERRILZED, SEEDED AND MILCHED, IF SOO IS USED NEXT TO SEEWALK OR SHARED-USE PATH, CONTRACTOR SHALL GRADE (TOPOL), ANDACENT TO THE SIDEMALK OR SHARED-USE PATH PRIOR TO PLACEMENT OF SOO TO INSURE THAT SOO IS PLACED FLUSH OR JUST BELOW EDGE OF SIDEWALK OR SHARED-USE PARTIO ANDOW DIMET PROMISED OR THE SIDEWALK OR SHARED-USE PARTIO.
- A 72-HOUR (MINIMUM) NOTICE SHALL BE GIVEN TO THE DELDOT DISTRICT PERMIT SUPERVISOR PRIOR TO STARTING UTILITY CONSTRUCTION.
- 20. A 48 HOUR NOTICE IS REQUIRED TO BE GIVEN TO THE DELDOT INSPECTOR PRIOR TO MATERIAL RELEASES.
- 21. ALL CONCRETE /HOT MIX MATERIALS SHALL BE RELEASED BY THE INSPECTOR PRIOR TO PLACEMENT.
- MISS UTILITY OF DELAWARE SHALL BE NOTIFIED THREE (3) CONSECUTIVE WORKING DAYS PRIOR TO EXCAVATION, AT 1-800-282-8555.
- 23. ALL SIGNING, STRIPING AND MAINTENANCE OF TRAFFIC IS THE CONTRACTOR'S RESPONSIBILITY AND SHALL FOLOW THE GUIDELINES SHOWN IN THE DELAWAGE MANUAL ON UNFORM TRAFFIC CONTROL DEVICES (DE MUTCD) FOR STREETS AND HIGHWAYS (LATEST EDITION). THE OWNER OR MAINTENANCE CORPORATION SHALL BE RESPONSIBLE FOR MAINTENANCE OF ALL SIGNS INSTALLED AS PART OF THIS PROJECT.
- 24. A COPY OF THE UP TO DATE APPROVED CONSTRUCTION DOCUMENTS AND DELDOT APPROVAL LETTERS SHALL BE MAINTAINED ON THE PROJECT SITE AT ALL TIMES AND BE AVAILABLE FOR INSPECTION BY DELDOT
- 25. EXISTING UTURES ARE SHOWN IN ACCORDANCE WITH THE BEST AWARDE INFORMATION, COMPLETEES OF THE UTURN COMPANIES INDUSTRIES. THE CHILD COMPANIES INDUSTRIES IN THE WORLD BE CONTINUED TO SERVICE THE UTURN COMPANIES INVOLVED IN ORDER TO SECURE THE WORST ACCORDATE INFORMATION AWARDED. AND CONSTRUCTION ARROUND OR ABACKET TO UTURIES SHALL BEEN WITHOUT MOTIFING THEIR OWNERS AT LESS 48-HOURS IN ADVINCE. THE CONTINUED SHALL BEEN THE THE OWNERS AT LESS 48-HOURS IN ADVINCE. THE CONTINUED SHALL BEEN THE ANY DAMAGE DODE TO THEM UT TO THIS, PERFORMENCE SHALL BE MEMBERLY AND COMPLETELY AND REPORT OF THE CONTINUED SHALL DRIVED STRENGT AND THE CONTINUED SHALL DRIVED SHALL DRIVE
- 26. SHOULD UTILITY RELOCATION BE REQUIRED, THE DEVELOPER MUST SUBMIT A UTILITY RELOCATION PLAN FOR DELOT REVEW, ALONG WITH CORRESPONDENCE FROM THE UTILITY COMPANIES STATING PRELIMINARY APPROVAL TO THE RELOCATION AND DESING OF THE UTILITIES PRIOR TO THE DELOT PRE-CONSTRUCTION METRIC, NO PHYSICAL CONSTRUCTION CAN COOK UNITL THE UTILITY PLANS ARE APPROVED. THE MONOVALUTHLY COMPANIES SUSEE FROM A PEPPORAL, AND A DELOTO UTILITY FROM SESSUE TO THE UTILITY COMPANIES SUSEE FROM A PEPPORAL, AND A DELOTO UTILITY FROM SESSUE TO THE UTILITY COMPANIES SUSEE FROM A PEPPORAL, AND A DELOTO UTILITY FROM THE SISSUE TO THE UTILITY.
- 27. DESIGN AND INSTALATION OF ALL PARTICUST MARRIAGE AND STREPHIG SHALL BE AS OUTLINED IN THE LATEST VERSION OF THE DE MAINTO, FOR FINAL PERMINENT PARAMENT MARRIAGES FROM TERMS POWER SHAME BE REQUIRED FOR LONG LIME STREPHIC. THERMO PLASTIC (CERTIDED OR PREFORMED MATERIAL) WILL BE REQUIRED ON ASPHALT SURFACES, FOR SHORT LUNG STREPHICA, LES STREMS LEST) WILL BE REQUIRED ON CONCRETE SURFACES, FOR SHORT LUE STREMMS, LE SYMBOLY, LICENDIG.
- BREAKAWAY POSTS SHALL BE USED WHEN INSTALLING ALL SIGNS. REFERENCE DELDOT STANDARD CONSTRUCTION DETAIL T-15.
- ALL PROPOSED CLOSED STORM DRAIN SYSTEMS SHALL BE VIDEO INSPECTED, REPAIRED AS NECESSARY AND APPROVED PRIOR TO THE INSTALLATION OF FINAL PAVING. IF REPAIRS ARE NEEDED, THE REPAIRED PIPE SECTIONS WILL NEED TO BE VIDEO INSPECTED AGAIN BEFORE THE REPAIR AND RE-APPROVED.
- 30. IT IS THE CONTRACTOR'S RESPONSIBILITY TO INSURE THAT PAVING WITHIN THE STATE OF DELAWARE RIGHT-OF-WAY IS INSTALLED TO THE ELEVATIONS SHOWN AND THAT NO PONDING OF WATER EXISTS AFTER PAVING IS COMPLETE.
- 31. THE DEPARTMENT RESERVES THE RIGHT TO STOP THE CONTRACTOR'S OPERATIONS, IF, IN THE OPINION OF THE DEPARTMENT'S REPRESENTATIVE, THE CONTRACTOR'S OPERATIONS ARE NOT IN COMPLIANCE WITH THE DELIMINE MUTCO, THE SPECIFICATIONS OR THE PLANS OR IF THE CONTRACTOR'S OPERATIONS ARE DEEMED MISSIVE.
- 32. ALL ROADWAY CLOSURES OR LANE CLOSURES BEYOND THOSE SPECIFIED AND APPROVED IN THE PLANS SHALL BE APPROVED BY THE DISTRICT SAFETY OFFICER A MINIMUM OF TWO WEEKS IN ADVANCE OF THE PROPOSED RESTRICTION.

- 33. TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE MAINTAINED IN GOOD CONDITION IN ACCORDANCE WITH THE BROCHINE ENTITLED "QUALITY GUIDELINES FOR TEMPORARY TRAFFIC CONTROL DEVICES", PUBLISHED BY THE AMERICAN TRAFFIC SERTY SERVICES ASSOCIATION (ASS). ANY TEMPORARY TRAFFIC CONTROL DEVICES THAT DO NOT MEET THE QUALITY GUIDELINES SHALL BE REMOVED AND REPLACED WITH ACCEPTABLE DEVICES. FAILURE TO COMPLY WILL RESULT IN WORK STOPPAGE.
- 34. THE CONTRACTOR SHALL PROVIDE ALL PROPERTY OWNERS AND RESIDENTS WHO LIKE ADJACENT TO THE WORK ZONE WITH WRITTEN NOTICE, 48 HOURS RO NAVIANCE OF THE START OF CONSTRUCTION WORK. THIS NOTIFICATION SHALL INCLUDE THE SCOPE OF WORK, WORKING HOURS, ANTIOPATED START AND COMPLETION DATES; A SUMMARY OF CONSTRUCTION ACTIVITIES WHICH HAV INTERFERE WITH ACCESS TO THE PROPERTY. INCLIDING A SCHEDILE MIN ACCESS CONSTINATION PLAN. CONTRICTOR'S MANE AND ADDRESS AND A BELDOT CONTRICT PHONE NUMBER, FALURE TO GIVE PROPER NOTICE WILL RESULT IN A SUSPENSION OF THE WORK REQUIRING NOTICE, UNTIL PROPER NOTICE IS PROVIDED. THE CONTRACTOR SHALL PROVIDE WRITTEN VERIFICATION TO THE ENDINGER THAT THE PROPERTY OWNERS AND RESIDENTS WERE NOTIFIED.
- 35. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING THE LOCAL 911 CENTER, LOCAL SCHOOLS AND THE DELDOT PUBLIC INFORMATION CENTER OF ALL ROADS AND LANES TO BE CLOSED A MINIMUM OF SEVEN CALENDAR DAYS BEFORE THE CLOSURE.
- THE CONTRACTOR SHALL NOTIFY THE LOCAL 911 CENTER IF ACCESS TO A FIRE HYDRANT IS TEMPORARILY RESTRICTED.
- 37. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT THE TRANSPORTATION MANAGEMENT CENTER IS NOTHED EACH AND DEFET DAY WHEN MORK IS BEING PERFORMED IN STATE RIGHT-OF-WAY. THE CONTRACTOR SHALL EDITING THE PICE OF WORK, AN INJECTS OF SECULDES SLOSED, THE LIGHT OF THE FOR WORK, WHEN THE LIME RESTRICTIONS ARE IN FACE AND WHEN LIME RESTRICTIONS ARE LIFTED. CONTRICT, PERSONCHIPMEN, MARKET AND STATE INSPECTION. THE TRANSPORTATION MANAGEMENT CONTRICT.
- AT THE END OF EACH WORKDAY, THE CONTRACTOR SHALL CORRECT ALL VERTICAL DIFFERENCES IN ACCORDANCE WITH TABLE 6G-1 OF THE DELAWARE MUTCD.
- 39. AT THE END OF EACH DAY'S OPERATION AND BEFORE TRAFFIC IS RETURNED TO UNRESTRICTED ROADWAY USE, TEMPORARY PAYEMENT MARKINGS SHALL BE APPLIED IN ACCORDANCE WITH THE DELAWARE MUTCD AND DELDON'S TEMPORARY PAYEMENT MARKINGS POLICY.
- 40. WHEN SIDE ROADS INTERSECT THE WORK ZONE, ADDITIONAL TRAFFIC CONTROL DEVICES SHALL BE ERECTED INCLUDING PERMANENT WARNING SIGNS.
- 41. ALL STORAGE OF EQUIPMENT AND MATERIAL SHALL COMPLY WITH SECTION 6G.21 OF THE DELAWARE MUTCO.
- 42. ALL FLAGGERS SHALL COMPLY WITH CHAPTER 6E OF THE DELAWARE MUTCO.
- 43. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING HIS/HER WORK WITH OTHER CONTRACTORS IN THE AREA.
- 45. ALL PAVEMENT MARKINGS THAT ARE NO LONGER IN USE AND CONFLICT WITH TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED AND COMPLETELY OBLITERATED BY A METHOD APPROVED BY THE ENGINEER. PAINTING OVER THE CONFLICTING PAVEMENT MARKINGS WILL NOT BE ACCEPTED AS A METHOD OF REMOVAL.











CROSSING EWES & I CANAL COUNTY, DELA WATER MAIN C OF THE LEW REHOBOTH (

-AWARE

JSSEX

LEGEND AND DELDOT UTILITY PLAN NOTES

SCALE	: NONE	SHEET NO.
DESIGN BY	: JWK	
DRAWN BY	: JWK	C1 1
CHECKED BY	: VAL,COD	G I. I
CAMB FILE	: 220183	
DATE	: AUGUST 2024	DRAWING 2 OF 19

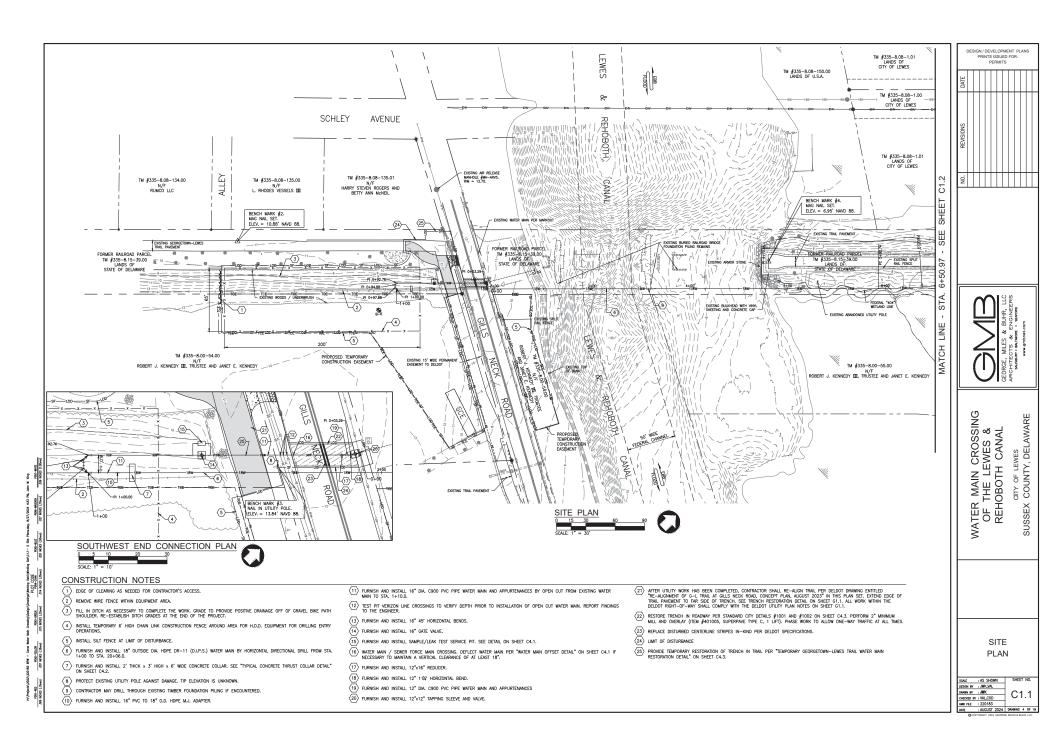
FDCS-CREDA 200 NOHES (25mm) MOES (18mm)

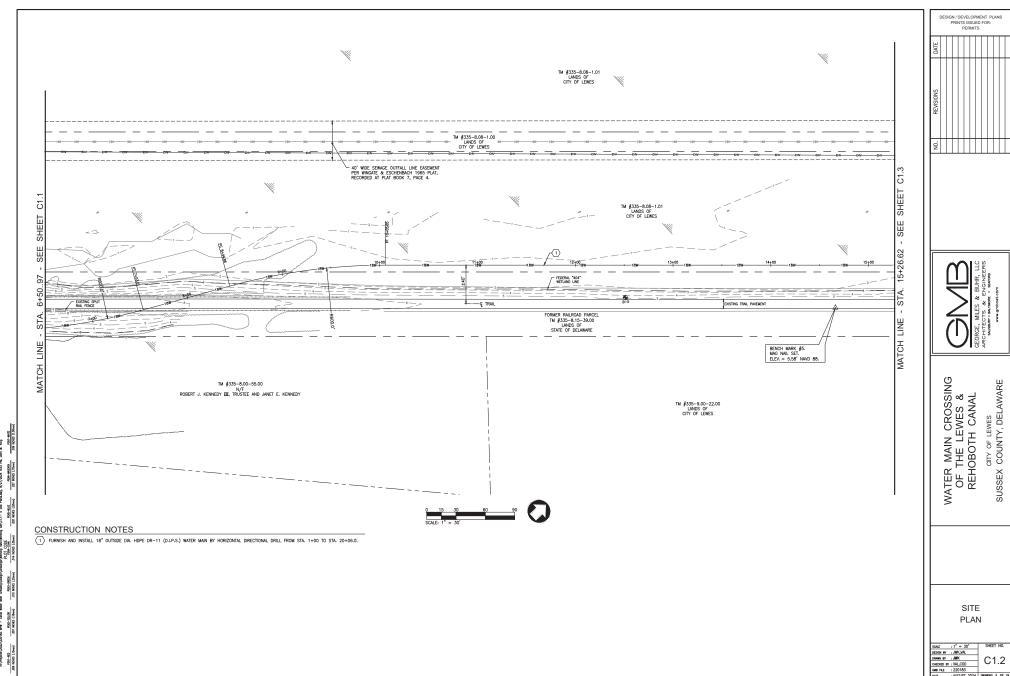
PDM-MADGA MOES (Jören)

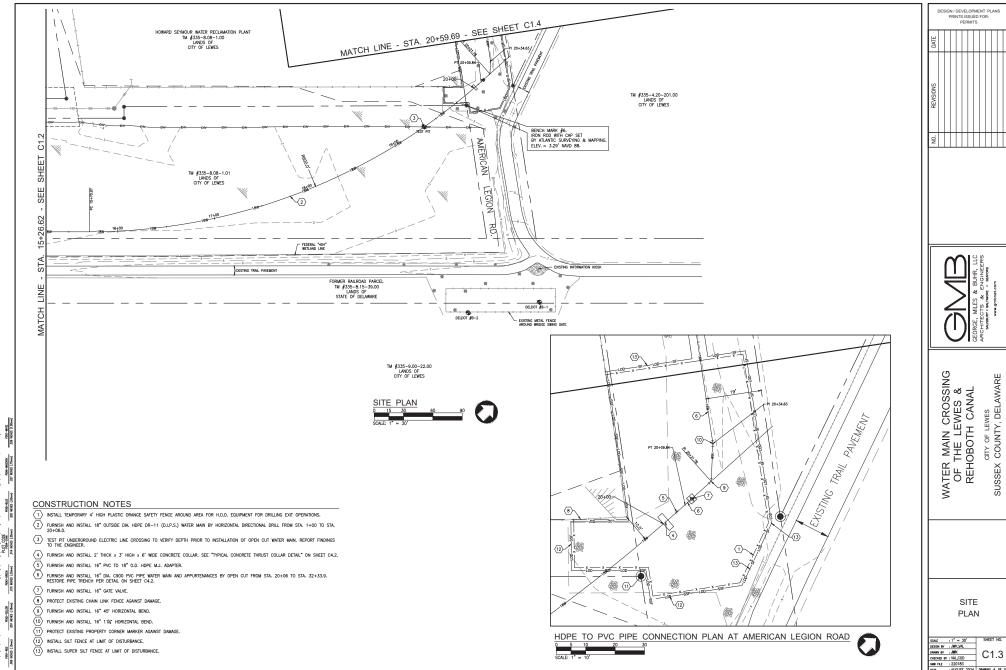
AZO NOES (50mm)

SHOW SHOW

DESIGN / DEVELOPMENT PLANS PRINTS ISSUED FOR: PERMITS Ö WATER MAIN CROSSING OF THE LEWES & REHOBOTH CANAL CITY OF LEWES SUSSEX COUNTY, DELAWARE KEY PLAN SOLE : 1" = 200" SHEET NO.
DESCN er : JWK,VAL
DEWEN er : JWK
GEDERD er : WL,CDD
GMB FILE : 220183
DATE : AUGUST 2024 DRAWNG 3 OF 19
COCCPRENT 2004 GEORGE MALES B BARR LIC







DESIGN / DEVELOPMENT PLANS PRINTS ISSUED FOR: PERMITS WATER MAIN CROSSING OF THE LEWES & REHOBOTH CANAL COUNTY, DELAWARE SUSSEX

> SITE PLAN

SOME : 1" = 30"
DESIGN BY : JWK,VAL
DRAWN BY : JWK
CHEKED BY : VAL,COD
GMB FILE : 220183 C1.4

CONSTRUCTION NOTES

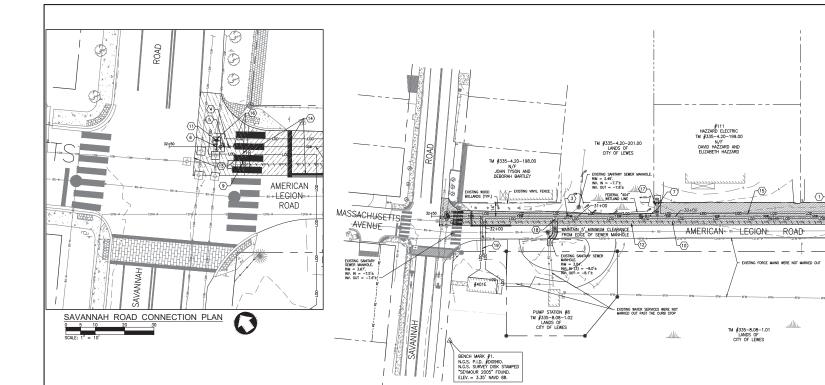
(a) CONNECT NEW 4" DUA. CARD PM. SERVICE PIPE TO DUSTING AT ONE FOOT CUISSE OF THE DUSTING FEMCE WITH THE PROPER FITTING AND ADMITTERS CONTINUED TO SHAUL FEED VERYOR COLORION, DETRIN MANTER AND THE METHERN OF DESIGNES WATER SERVICE PIPE. REJUNCE THE SAME DUMETER SERVICE PIPE. REJUNCE THE SAME DUMETER SERVICE PIPE. REJUNCE THE SAME DUMETER SERVICE PIPE. REJUNCE THE SERVICE PIPE. REJUNCE THE SERVICE PIPE. REJUNCE THE SERVICE PIPE. REJUNCE THE SERVICE PIPE. SERVICE PIPE SERVICE PIPE SERVICE PIPE.

9 PERFORM 1½" MINIMUM MILL AND OVERLAY (SUPERPAVE TYPE C, 1 LIFT) IN CITY RIGHT-OF-WAY. PHASE WORK TO ALLOW ONE-WAY TRAFFIC AT ALL TIMES.

(10) TEST PIT EXISTING 12" SEWER FORCE MAIN NEXT TO WATER LINE CROSSING TO VERIFY FORCE MAIN DEPTH PRIOR TO INSTALLATION OF WATER SERVICE AND HYDRANT LEAD. REPORT FINDINGS TO THE ENGINEER.

(11) LIMIT OF DISTURBANCE.

 $\fbox{12}$ Install silt fence at limit of disturbance.



CONSTRUCTION NOTES

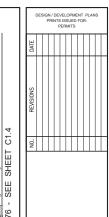
PDIS-MICDEA WDES (JThrm)

- TO FURNISH AND INSTALL 16" DIA. C900 PVC PIPE WAITER MAIN AND APPURTENANCES BY OPEN CUT FROM EXISTING WATER MAIN TO H.D.D. STA. 20+06.0. RESTORE PIPE TRENCH PER DETAILS ON SHEETS G1.1 AND C4.3, AS APPUCABLE.
- (2) WAITER MAIN / SEWER LATERAL CROSSING, DEFLECT WAITER MAIN PER "WAITER MAIN OFFSET DETAIL" ON SHEET C4.1 IF NECESSARY TO MAINTAIN A VERTICAL CLEARANCE OF AT LEAST 18".
- 3 WATER MAIN / SEWER MAIN CROSSING. DEFLECT WATER MAIN PER "WATER MAIN OFFSET DETAIL" ON SHEET C4.1 IF NECESSARY TO MAINTAIN A VERTICAL CLEARANCE OF AT LEAST 18". SEE "WATER MAIN AT SEWER MAIN CROSSING PROFILE" ON SHEET C3.2.
- 4 FURNISH AND INSTALL 16"x12" REDUCER.
- 5 FURNISH AND INSTALL 12" DIA. C900 PVC PIPE WATER MAIN AND APPURTENANCES.
- 6 FURNISH AND INSTALL 12"x12" TAPPING SLEEVE AND VALVE. SEE "GATE VALVE DETAIL" ON SHEET C4.1.
- COMMERCIAL BULDING WATER SERVICE REMOVE THE EXISTING WATER SERVICE PIPE AND CURB STOP BETWEEN THE CONNECTION AT THE NEW METER PI AND THE DISTING WATER WATER, REMOVED RESIDENT, ASSTUL, NEW SPACE WATER WETER PIT, CURB STOP DEPTH, DUMENTER AND PIPE WATER AND FEW METER PIT, FORMES AND SETTERS SHALL BE PURCHASED BY LEWES BOARD OF PUBLIC WORKS, INSTALL PER "TYPICAL HOUSE SERVICE CONNECTION DETAIL" ON SHEET CALE.
- (8) COMECT NOW P.S. SEPINCE, PIEC (1" DIE, MINISHAM) TO DESTINE WITH THE PROPER FITTINGS AND ADJETERS. CONTRACTOR SHILL FILED VERDITY LOCATION, DEPTH, DAMETER AND PIECE MESTINES WERE SENSING WITH SWINGER AND SHILL PROPULE THE SWING CHARMET SERVICE FOR EXCENSING CHARMET SERVICE THE COSTRING WERE PREVIOUS THE CONTROL FOR STOP, PREVIOUS THE EXISTING WITH SERVICE PIECE STRINGS AND INSTALL NEW CLARS STOP, REMOVE THE EXISTING WITHER THAN THE COSTRING WERE THE ADJET STOP WITH ADDITION THE STOP OF THE COSTRING WERE THE ADJET STOP OF THE ADJET STOP OF THE COSTRING WERE THE ADJET STOP OF THE COSTRING WERE THE ADJET STOP OF THE
- 9 CUT AND PLUG EXISTING WATER MAIN. SEE "PLUGGING ABANDONED LINES DETAIL" ON SHEET C4.1.
- (10) ABANDON EXISTING 6" DIA. CAST IRON WATER MAIN IN-PLACE.
- 11) FURNISH AND INSTALL 12" INSERTION VALVE. SEE "GATE VALVE DETAIL" (FOR CONCEPT) ON SHEET C4.1.

12 PERFORM 1/2" MINIMUM MILL AND OVERLAY (SUPERPAVE TYPE C, 1 LIFT) IN CITY RIGHT-OF-WAY. PHASE WORK TO ALLOW ONE-WAY TRAFFIC AT ALL TIMES.

SITE PLAN

- (13) EDGE OF DELDOT MILL AND OVERLAY. SEE SHEET G1.1.
- (14) REPLACE DISTURBED CROSSWALK STRIPES, CENTERLINE STRIPE AND STOP BAR IN-KIND PER DELDOT SPECIFICATIONS.
- (15) LIMIT OF DISTURBANCE
- (16) RESTORE PIPE TRENCHES IN DELDOT RIGHT-OF-WAY WITH DEEP LIFTS PER "PERMANENT CROSS-ROAD OR LONGITUDINAL PATCH DETAIL" ON SHEET G1.1.
- (17) APARTMENT BULDING WATER SERVICE: REMOVE THE EXISTING WATER SERVICE PIPE AND CURB STOP (NOT FOUND) BETWEEN THE CONNECTION AFT THE NEW METER HI AND THE EXISTING WATER MAIN. REMOVE EXISTING WATER WATER HIS RETURN BULDING SEASONIT, INSTALL HER SYNCLE WATER METER HI, CURB STOP MAD PLE SERVICE PIPE (2" DAY MANNAMI) TO EXISTING WHITH THE PROPER HITMINS AND AUMPTERS. CONTRACTORS SHALL PIELD VERTILE AND AUMPTERS. CONTRACTORS SHALL PIELD WATER AS AND SETTEMS SHALL EVEN FOR SHALL PRESENT AND SHALL SHALL SHALL BE SERVICE PIPE WATER AS AND SETTEMS SHALL EVEN SHALL BE PURCHASED BY LEWS BOARD OF PUBLIC WORKS. MISTALL PET "PIPCAL FLOWS SERVICE CONNECTION.
- (8) REMOVE THE EXISTING WATER SERVICE PIPE BETWEEN THE CONNECTION POINT AND THE EXISTING WATER MAIN. INSTALL NEW P.E. SERVICE PIPE (1" DA MINMANN) TO EXISTING WATER SERVICE PIPE (BIT INCS AND ADAPTERS, CONTRACTOR SHALL PIEJD VERIEV LOCATION, DEPTH, DIMMETER AND PIPE MATERIAL OF EXISTING WATER SERVICE AND SHALL PROVIDE THE SAME DIAMETER SERVICE PIPE. INSTALL PER "TYPICAL HOUSE SERVICE CONNECTION DETAIL" ON SHEET C4.2.
- (19) INSTALL NEW SINGLE WATER METER PIT, CURB STOP AND P.E. SERVICE PIPE (1" DIA. MINMAM) TO EXISTING SERVICE IN/LINDER BUILDING WITH THE PROPER TRITINGS AND AMPETERS. CONTRACTOR SHALL FIELD VERFY LOCATION, DEFIH, DUMETER AND PIPE METERAL PO'C EXISTING WATER SERVICE AND SHALL PROVIDE THE SHALE DIMETER SERVICE PIPE INSTALL PER "PIPCHA INSTALL SERVICE CONNECTION DETAIL" OF SHEET SHALE PROVIDE THE SHALE PROVIDED THE SHALE PROVIDED



#113 TM #335-4.20-200.00 N/F AMERICAN LEGION HOME INC.,

LEWES POST #17

(8)

GEORGE MILES & BUHR, LLC
ARCHITECTS & ENGINEERS
SUSTAINED: BLUMBE : SURVEY

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INE INE

MATCH

WATER MAIN CROSSING OF THE LEWES & REHOBOTH CANAL ADD OF LEWES SUSSEX COUNTY, DELAWARE

> SITE PLAN

SOME : AS SHOWN
DESIGN BY : JWK,VAL
DRAWN BY : JWK
CHEINED BY : VML,COD
GMB FILE : 220183

: AUGUST 2024 | DRAWING 8 OF COPYRIGHT 2024 GEORGE, MLES & BUHR, LLC

C1.5

SEDIMENT CONTROL GENERAL NOTES

- SUSSEX CONSERVATION DISTRICT MUST BE NOTIFIED IN WRITING FIVE (5) DAYS PRIOR TO COMMENCING WITH CONSTRUCTION. FAILURE TO DO SO CONSTITUTES A VIOLATION OF THE APPROVED SEDIMENT AND STORMATER MANAGEMENT PLAN.
- 2. REVIEW AND OR APPROVAL OF THE SEDIMENT AND STORMWATER MANAGEMENT PLAN SHALL NOT RELIEVE THE CONTRACTOR FROM HIS OR HER RESPONSIBILITIES FOR COMPLANCE WITH THE REQUIREMENTS OF THE DEVLAMME SEDIMENT AND STORMMATER RECOL
- IF THE APPROVED PLAN NEEDS TO BE MODIFIED, ADDITIONAL SEDIMENT AND STORMWATER CONTROL MEASURES MAY BE REQUIRED AS DEEMED NECESSARY BY THE SUSSEX CONSERVATION DISTRICT.
- 4. FOLLOWING SOIL DISTURBANCE OR REDISTURBANCE, PERMANENT OR TEMPORARY STABILIZATION SHALL BE COMPLETED FOR ALL PERMITER SEDIMENT CONTROLS, SOIL STOCKPIELS, AND ALL OTHER DISTURBED OR GRADED AREAS ON THE PROJECT SITE WITHIN 14 CALENDAR DAYS UNLESS MORE RESTRICTIVE FEDERAL REQUIREMENTS APPLY.
- ALL EROSION AND SEDIMENT CONTROL PRACTICES SHALL COMPLY WITH THE DELAWARE EROSION AND SEDIMENT HANDBOOK, LATEST EDITION.
- AT ANY TIME A DEWATERING OPERATION IS USED, IT SHALL BE PREVIOUSLY APPROVED BY THE AGENCY CONSTRUCTION SITE REVIEWER FOR A NON-ERGSIVE POINT OF DISCHARGE, AND A DEWATERING PERMIT SHALL BE APPROVED BY THE DNREC WELL PERMITTING BRANCH.
- APPROVAL OF A SEDIMENT AND STORMMATER MANAGEMENT PLAN DOES NOT GRANT OR IMPLY A RIGHT TO DISCHARGE STORMMATER RUNOFF. THE OWNER/DEVELOPER IS RESPONSIBLE FOR ACQUIRING ANY AND ALL AGREEMENTS, ESCHOOLS, RECESSARY TO COMPLY WITH STATE DRAINAGE AND OTHER APPLICABLE LAWS.
- THE CONTRACTOR SHALL AT ALL TIMES PROTECT AGAINST SEDIMENT OR DEBRIS LADEN RUNGF OR WIND FROM LEARNO THE SITE. PERMETER CONTROLS SHALL BE CHECKED BUT AND ADMIT AND ADMITS SHADOWN SHALL BE CHECKED BUT AND ADMITS AND CONTROL SEDIMENT FROM LEVING THE SITE. ACCOUNTANTED SEDIMENT SHALL BE REVOLVED THEN IT THE CONTROL SEDIMENT SHALL BE CHEVROWN THEN IT THE CONTROL OF ALTER MEXISTERS IN TIMES OF ADMERSE WEATHER CONDITIONS, OR AS DIRECTED BY THE AGENCY CONSTRUCTION SITE REVUEWER.
- BEST AVAILABLE TECHNOLOGY (BAT) SHALL BE EMPLOYED TO MANAGE TURBID DISCHARGES IN ACCORDANCE WITH REQUIREMENTS OF 7. DEL C. CH 60, REQUIATIONS COVERNING THE CONTROL OF WATER POLLUTION, SECTION 9-1.02, KNOWN AS SPECIAL CONDITIONS FOR STORMANTER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITIES, MAD DEPARTMENT POLICIES, RAPIO CUBUNES, AND COURSE.

SEQUENCE OF OPERATIONS

A PRE-CONSTRUCTION MEETING SHALL BE HELD PRIOR TO COMMENCEMENT OF SITE CONSTRUCTION. ALSO, THE SUSSEX CONSERVATION DISTRICT SHALL BE NOTIFIED IN WRITING FIVE (5) DAYS PRIOR TO COMMENCING WITH CONSTRUCTION. FAILURE TO DO SO CONSTITUTES A VIOLATION OF THE APPROVED SEDMENT AID STORM WATER MANAGEMENT FLAN.

PHASE 1:

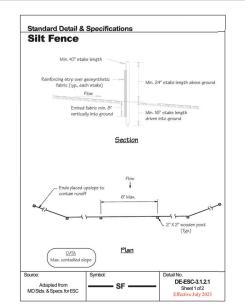
- INSTALL STABILIZED CONSTRUCTION ENTRANCE AND PERIMETER CONTROLS IN THE VICINITY OF THE ENTRY POINT AREA.
- 3. INSTALL PERIMETER CONTROLS IN THE VICINITY OF THE EXIT POINT AREA.
- 4. CLEAR AND GRUB AREA NEEDED FOR CONTRACTOR'S EQUIPMENT.
- 6. INSTALL TEMPORARY CONSTRUCTION FENCE.
- PERFORM HORIZONTAL DIRECTIONAL DRILLING. BORE FROM THE SOUTHERN END.
- PULL WATER MAIN PIPE FROM THE SOUTHERN END. PIPE FUSING AND PIPE LAYOUT SHALL BE IN THE AMERICAN LEGION ROAD RIGHT-OF-WAY.
- 9. BACKFILL PIT FOR DRILLING MUD AT THE EXIT POINT AREA.
- INSTALL WATER MAIN AND APPURTENANCES TO CONNECTION POINT TO THE EXISTING WATER MAIN ALONG GILLS NECK ROAD, BACKFILL TRENCHES AND RESTORE ROAD PAYMENT.

PHASE 2:

- 11. REMOVE PORTION OF EXISTING PAVED TRAIL AT INTERSECTION WITH GILLS NECK
- 12. INSTALL PCC TRAIL ENTRANCE RAMP
- 13. INSTALL RE-ALIGNED HOTMIX TRAIL.
- TOPSOIL AND RESTORE ALL DISTURBED GRASS AREAS IN THE ENTRY POINT AND TRAIL RE-ALIGNMENT AREAS AND VEGETATE AS REQUIRED.

PHASE 3:

- INSTALL BY OPEN CUT WATER MAIN PIPE AND APPURTENANCES, FROM THE HDD EXIT POINT TO THE EXISTING WATER MAIN ALONG SAVANNAH ROAD. BACKFILL TRENCHES AND RESTORE ROAD SURFACES.
- REMOVE PROJECT SEDIMENT CONTROL MEASURES AND STABILIZE AREA AFTER OBTAINING PERMISSION FROM SEDIMENT CONTROL INSPECTOR.



Chain link fence with

Perspective

Section

SSF

Symbol

33" min, post and 2M layer geotextile fab

16" min. 1st laye

geotextile fairle

DATA

DE-ESC-3.1.2.3

Sheet 1 of 2 Effective July 2023

aeotextile fabric

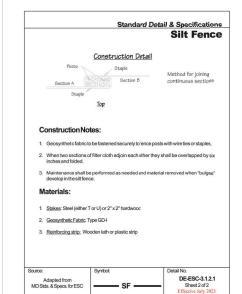
Chain link fencina -Georextile fabric

into ground

Adapted from

MD Stds. & Specs. fcr ESC

Source



Standard Detail & Specifications Standard Detail & Specifications **Super Silt Fence** Super Silt Fence **Construction Notes:** 33" Min. 1. The poles do not need to be set in concrete. 2. Chain link fence shall be fastened securely to the fence posts with wire ties or staples Flow 2-1/2" dia. aal/anized 3. Geotextile fabric shall be fastened securely to the chain link fence with ties spaced

8" Min. 4. Geotextile fabric shall be embedded a minimum of 8" into the ground. 5. When two sections of geotextile fabric adjoin each other, they shall be overlapped by

> 6. Maintenance shall be performed as needed and silt buildups removed when "bulges" develop in the silt fence.

- 1. Fencing: Fencing shall be 42 inches in height and constructed in accordance with the latest Delaware Department of Transportation (Del-DOT) Specifications for Chain Link Fencing (Section 727). The Del-DOT specification for a 6 foot fence shall be used, substituting 42 inch fabric and 6 foot length posts.
- 2. Geosynthetic Fabric: Type GD-I

every 24" at the top and mid section

Source:	Symbol:	Detail No.
Adapted from MD Stds. & Specs. for ESC	ssf	DE-ESC-3.1.2.3 Sheet 2 of 2 Effective July 2023

PROJECT INFORMATION

PARCEL INFORMATION

TAX MAP 335-8.15-39.00 (FORMER RAILROAD PARCEL) TAX MAP 335-8.00-54.00 IAX MAP 335-8.00 GILS NECK ROAD (PUBLIC RIGHT-OF-WAY)
TAX MAP 335-8.00 GILS NECK ROAD (PUBLIC RIGHT-OF-WAY)
TAX MAP 335-8.08-1.01
TAX MAP 335-0.88-1.00
TAX MAP 335-4.20 AMERICAN LEGION ROAD (PUBLIC RIGHT-OF-WAY)

AREA TO BE DISTURBED: EXISTING PERVIOUS AREA WITHIN L.O.D.: 18.055 SO FT PROPOSED (POST_CONSTRUCTION) PERMOUS AREA WITHIN LOID . 18 197 SO ET EXISTING IMPERVIOUS AREA WITHIN L.O.D.: 11.482 SQ, FT, PROPOSED (POST-CONSTRUCTION) IMPERVIOUS AREA WITHIN L.O.D.: 11,340 SQ. FT.

SITE ADDRESS / LOCATIONS OF PARCELS WITH PROPOSED SURFACE DISTURBANCE (ALL IN LEWES, DELAWARE):

TOTAL AREA OF PARCELS AND AFFECTED RIGHT-OF-WAYS: 18.81 ACRES

CONTACT INFORMATION

PROJECT OWNER: LEWES BOARD OF PUBLIC WORKS 107 FRANKLIN AVENUE LEWES, DE 19958

CONTACT: AUSTIN CALAMAN (302) 645-6228 (PHONE) (302) 645-6358 (FAX)

GEORGE, MILES & BUHR, LLC 400 HIGH STREET SEAFORD, DE 19973

CHARLES M. O'DONNELL. P.E. CONTACT: (410) 430-7056 (PHONE) (302) 628-8350 (FAX)

OWNER'S CERTIFICATION

I, THE UNDERSIGNED, CERTIFY THAT ALL LAND CLEARING, CONSTRUCTION AND DEVELOPMENT SHALL BE DONE PURSUANT TO THE APPROVED PLAN AND THAT DEVELOPMENT SHALL BE DONE PURSUANT TO THE APPROVED PLAN AND THAT RESPONSIBLE PERSONNEL (C., BLUE CARE HOLDER) INVOLVED IN THE LAND DISTURBANCE WILL HAVE A CERTIFICATION OF TRANSIC PRIOR TO MINISTON FOR THE CONTROL OF TRANSIC PRIOR TO MINISTON HOLDER TO THE CONTROL OF ERSONN AND STREAM THE DIRECT SEMBENT AND STORMANCE PROGRAM AND/OR THE RELEVANT DELCATED ARROY TO RESPONSIBILITIES UNDER THE NOTES CONSTRUCTION AND AND UNDERSTAND AN RESPONSIBILITIES UNDER THE NOTES CONSTRUCTION GENERAL PERSONN AND REPORTED ON THIS CONTROL THE NOTES CONSTRUCTION

AUSTIN CALAMAN, GENERAL MANAGER LEWES BOARD OF PUBLIC WORKS

DATE

DESIGN / DEVELOPMENT PLANS ě CROSSING EWES & I CANAL WATER MAIN CO OF THE LEW REHOBOTH (

COUNTY, DELAWARE SUSSEX

EROSION AND SEDIMENT CONTROL NOTES AND

DETAILS SCALE : AS NOTED DESIGN BY : JWK
DRAWN BY : JWK
CHECKED BY : VAL,COD
GMB FILE : 220183 C2.1 : AUGUST 2024 DRAMING 9 OF

PENE-MACENTA

- a. Straw-Straw shall be unrotted small grain straw applied at the rate of 1-1/2 to 2 tons per acre, or 70 to 90 punds (two bales) per 1,000 equate feet. Much materials shall be relatively free of weeds and shall be free of motions weeds such as threets, Johnsongrass, and gaskgrass. Straw mich uniformly by hard of mechanically for uniform distribution of hard spread much, divide sea will opportunished 1,000 square feet actions and place 17-200 punctive (be raise) of much in each section.
- b. Wood of pips.-Apply at the rate of approximately 6 tons per acre or 775 pounds per 1,000 square feet when walkable and when hasable. These are particularly well suited for stilly and road rights-of-way. If wood chaps are used, increase the application nate of intropen feetings tracy 200 pounds for per acre (200 pounds of 10-10-10 or 65 pounds of 30-00 per acre). If yellow the period of the period of
- i. Definitions:
 - a. Wood fiber mutch shall consist of specially prepared wood that has been processed to a uniform state, is packaged for sale as a hydraulic mutch for use with hydraufic seeding equipment, and consists of a minimum of 70% virgir or recycled wood fiber combined with 30% paper fiber and additives.
 - 50% pages (file and additives. Blanded file minds had consist of any hydraulic nuclei that contains greater than 30% pages files. The pages component must consist of specially proposed pages that has been with hydraulic second propiations. A bonded file minds (BRI) consists of long steam, specially prepared used the hydraulic second as uniformation of the second propiations of the A bonded file minds (BRI) consists of long steam, specially prepared used files that here been processed as a uniformation belongwish by a water resistant bonding agent. BRIs data location to a paper (calculation) must be in many contain mind precordings of the page of the second propiation of the pages of the second propiation and the second propiation of the pages of the second propiation and the second propiation th

 - d. Refer to Figure 3.4.5a for conditions and limitations of use for each of the above categories
- All components of the hydraulically applied mulches shall be pre-packaged by the assure material performance. Field mixing of the mulch components is acceptable, per manufacturers recommendations to ensure the proper results.
- Hydraulically applied mulches and additives shall be mixed according to manufacturers recommendations.
- Materials within this category shall only be used when hydraulcally applied mulch has been specified for use on the approved Sediment and Stormwater Plan, o supplemental approval from the pla approval agency has been obtained in writing for a specificarea.

urce:	Symbol:	Detail No.
		DE-ESC-3.4.5
elaware ESC Handbook & Filtrexx™ International		Sheet 1 of 3
x Filtrexx - International		Effective July 2023

Standard Detail & Specifications

Mulching

- Applications:

 A Apply product to geolechnically stable stopes that have been designed and constructed to diver nordl eavy from the face of the stope.

 Do not apply to seasted solve, or gregorithm is satisfacted within 24-48 hours.

 Do not apply to seasted solve, or gregorithm is an indipated within 24-48 hours.

 Do not apply to seasted solve, or gregorithm is a find-patent or to liverance 30 seasters.

 Do not apply to seasters and the seasters of the seaste

- Sec. Che. Mis real popy seed and sol amendments with a small amount of much for your infection.

 Size. Two. Mis and apply much in manufacturers recommended rates over feetily.

 Size. Two. Mis and apply much in manufacturers recommended rates over feetily.

 Size. Two. Mis and apply much in opposing directions to achieve optimum soid coverage.

 e. Minimum currier temperatures as 40°F £4°C.). The best results and more repict urring war and continued to the programment of the prog
- Anchoring mutch Nutch must be anchored immediately be minimize loss by wind or veter. This may be bone by one of the following methods, depending upon size of area, erosion hazard, and cost. S. Chrighing A crimpre is arbard return implement designed to punch and another mutch into the top two (2) inches of soil. This practice affectes maximum erosion control but is limited to futilities to equipment can openet safety. On stopping land, crimpring should be done on the control when the equipment can openet safety. On stopping land, crimpring should be done on the control when the part of the control is suffered to the control when the control when the control when the part of the control is suffered to the control when the control when the part of the control when the control when the control when the part of the control when the control when the part of the control when the control when the part of the control when the control when the part of the control when the control when the part of the control when the control when the part of the part of the control when the part of part of
- possible. Tracking is the process of calling much (usually steen) into the soil using a buildozer or other experiment that must not buildouties. Tracking is used primary on slopes. 31 or steeper and should Liquid much brighter and process of the steeper and the steep of the steeper and should Liquid much brighter. Applications of slope, and much brighters should be heavier at despire, in walleys, and at creats of banks and other areas where the much must be moved by wind or water. All other areas should have a unaform application of bridge. The use of synthetic buildings is the preferred method of much have a unaform applicant of bridge. The use of synthetic buildings is the preferred method of much the second of the second of the second second of the sec
- binding and should be applied at the rates recommended by the manufacturer.

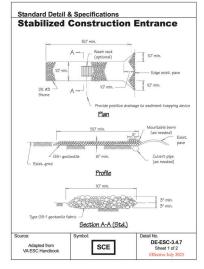
 Paper fiber The fiber brinder shall be applied at a net dry weight of 750 besics. The wood cellulose fibe shall be mixed with water, and the mixture shall contain a maximum of 50 bs. of wood cellulose fiber pe

100 gations.

Nettings - Biodegradable nettings may be used to secure straw mulch. Install and secure according to the manufacturer's recommendations. Photodegradable or synthetic nettings are not acceptable.

Source:	Symbol:	Detail No.
		DE-ESC-3.4.
Delaware ESC Handbook		Sheet 2 of 3
& Filtrexx™ International		Effective July 200

Standard Detail & Specifications Mulching ***** ****||***||****||****||*** Symbol DE-ESC-3 4 5 Sheet 3 of 3 Effective July 2023 & Filtrexx™ International



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WATER MAIN CROSSING OF THE LEWES & REHOBOTH CANAL

CITY OF LEWES COUNTY, DELAWARE

SUSSEX

DESIGN / DEVELOPMENT PLANS

Standard Detail & Specifications Stabilized Construction Entrance



Section A-A (Opt.) Construction Notes:

- 1. Stone size Use DE #3 stone.
- Length As required, but not less than 50 feet (exception a single residence lot where a 30 foot minimum length would apply).
- 3. Thickness Not less than size (6) inches.
- 4. Width Ten (10) foot minimum, but not less than the full width at points where ingress or egress
- 5. Geotextile Type GS-I; placed over the entire area prior to placing of stone.
- <u>Surface Water</u> All surface water flowing or diverted toward construction entrances shall be piped across the entrance. If piping is impractical, a mountable berm with 5:1 slopes will be permitted.
- 7. Maintenance The entrance shall be maintained in a condition which will prevent tracking or flowing of sediment onto public rights of-way. This may require periodic top dressing with additional stone as conditions demand and repair and/or cleanout of any measures used to trap sediment. All sendent spilled, dropped, washed or tracked onto public rights-of-way must be removed immediately.
- Washing Vehicle wheels shall be cleaned to remove sediment prior to entrance onto public rights-of-way. When washing is required, it shall be done on an area stabilized with stone and which drains into an approved sediment trapping device.
- 9. Inspection Periodic inspection and needed maintenance shall be provided after each rain

Source:	Symbol:	Detail No.
Adapted from VA ESC Handbook	SCE	DE-ESC-3.4 Sheet 2 of 2 Effective July 2

Standard Detail & Specifications **Dust Control**

Temporary Methods:

- 1. Mulches See DE-ESC-3.4.5, Standard Detail and Specifications for Mulching.
- Vegetative cover See DE-ESC-3.4.3, Std. Detail and Specifications for Vegetative Stabilization.
- Adhesives Use on mineral soils only (not effective on muck soils). Keep traffic off these The following table may be used for general guidance.

Type of Emulsion	Water Dilution	Type of Nozzle	Apply Gal/Ac.
Latex emulsion	12.5:1	Fine spray	235
Resin-in-water emulsion	4.1	Fine spray	300
Acrylic emulsion (non-traffic)	7:1	Coarse spray	450
Acrylic emulsion (traffic)	3.5:1	Coarse spray	350

- 4. Tillage For emergency temporary treatment, scarify the soil surface to prevent or reduce the amount of blowing dust until a more appropriate solution can be implemented. Begin the tillage operation on the windward side of the site using a chisel-type plow for best results.
- 5. Sprinkling Sprinkle site with water until the surface is moist . Repeat as needed.
- Calcium Chloride Apply as flakes or granular material with a spreader at a rate that will keep the soil surface moist. Re-apply as necessary.
- Barriers Place barriers such as solid board fences, snowfences, hay bales, etc. at right angles to the prevailing air currents at intervals of approx. 10X their height.

Permanent Methods:

- Vegetative cover See DE-ESC-3.4.3, Std. Detail and Specifications for Vegetative Stabilization.

Stone - Apply layer of crushed stone or coarse gravel to protect soil surface.				
Source:	Symbol:	Detail No.		

DF-FSC-3.4.8 Adapted from VA ESC Handbook Effective July 2023

Standard Detail & Specifications

Construction Site Pollution Prevention

Delaware NPDES Discharge Permit

((Project Name))

((NOI Permit Number))

((Contact Name & Number for Additional Site Information))

((Contact Name & Number to Obtain Copy of Approved Plan))

If you observe indicators of stormwater pollutants in the discharge or in the receiving waterbody, call the DNREC Spill Notification 24 HR Hotline at

1-800-662-8802

Example Construction General Permit (CGP) Signage

- Minimum elan elze 2' x 2'

- 2. Minimum text eize! "
 3. Gign must be posted at a safe, publicly accessible location close to construction site
 4. Sign must be visible from the public road nearest the active construction site
 5. Signe posted within a DelDOT or other public road nearest eighth-of-way (ROM) must be in accon
 with all local and/or State requirements in regards to safety, location, orientation, etc.

II Ce.	Symbol.	Detail No.
		DE-ESC-3.6.1
elaware ESC Handbook		Sheet 1 of 4
		Effective July 2023

Standard Detail & Specifications **Construction Site Pollution Prevention**

Notes:

The Construction Site Pollution Prevention Plan includes the following elements:

1. Material Inventory

Document the storage and use of the following materials

- a. Concrete
- c. Paints (enamel and latex)
- d. Cleaning solvents
- e. Pesticides
- f. Wood scraps
- g. Fertilizers
- h. Petroleum baset products 2 Good housekeeping practices
- a. Store only enough product required to do the job.
- b. Store all materials in a neat, orderly manner in their original labeled containers and

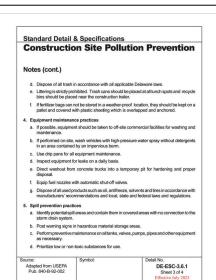
- d. When possible, use all of a product prior to disposal of the container
- e. Manufacturers' instructions for disposal should be strictly adhered to.
- f. Designate someone to inspect all BMPs daily. 3. Waste management practices
- a. Collect and store all waste materials in securely lidded dumpsters in a location that does
- Salvage and/or ecycle waste materials whenever possible.
- c. The dumpsters shall be emptied a minimum of twice perweek, or more if necessary. The

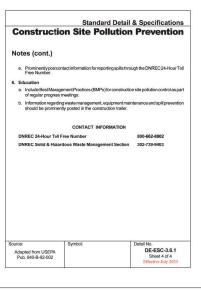
licensed trash hauler is responsible for cleaning out dumpsters.					
Source: Adapted from USEPA Pub. 840-B-92-002	Symbol:	Detail No. DE-ESC-3.6.1 Sheet 2 of 4			

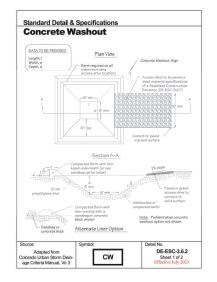
EROSION AND SEDIMENT CONTROL

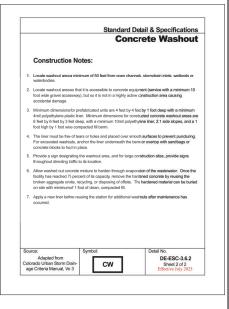
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DESIGN / DEVELOPMENT PLANS

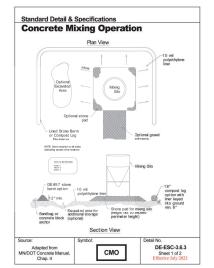
WATER MAIN CROSSING OF THE LEWES & REHOBOTH CANAL CITY OF LEWES COUNTY, DELAWARE

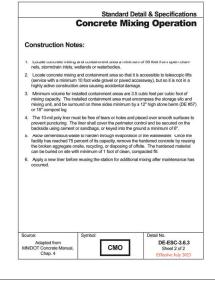
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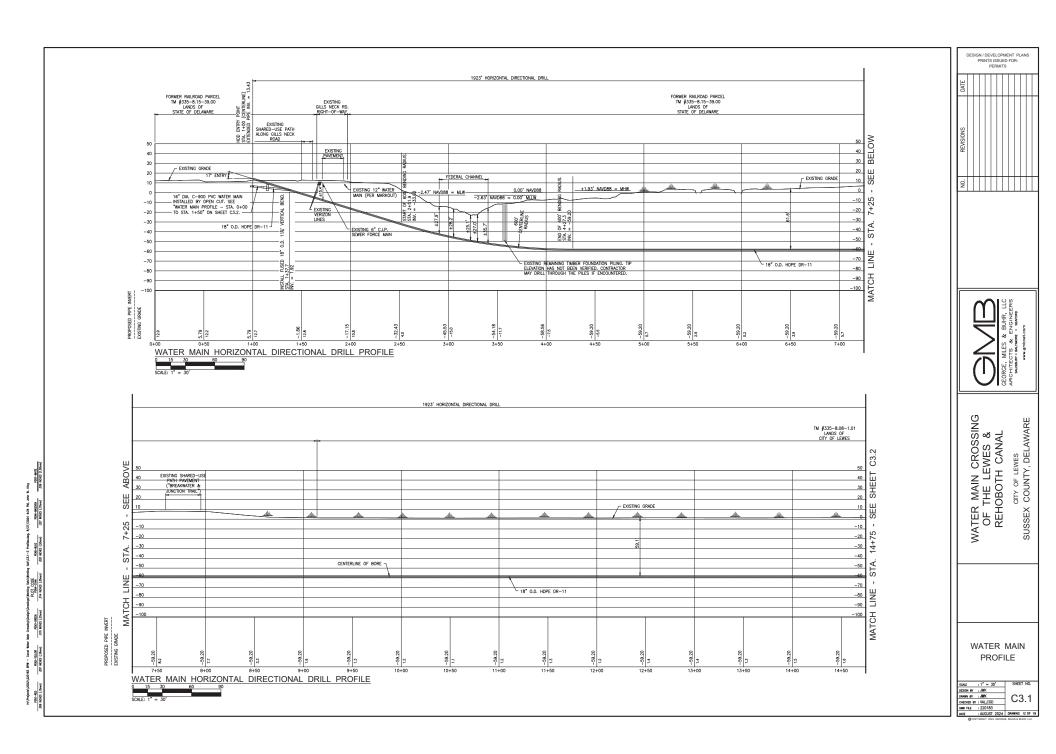
EROSION AND SEDIMENT CONTROL

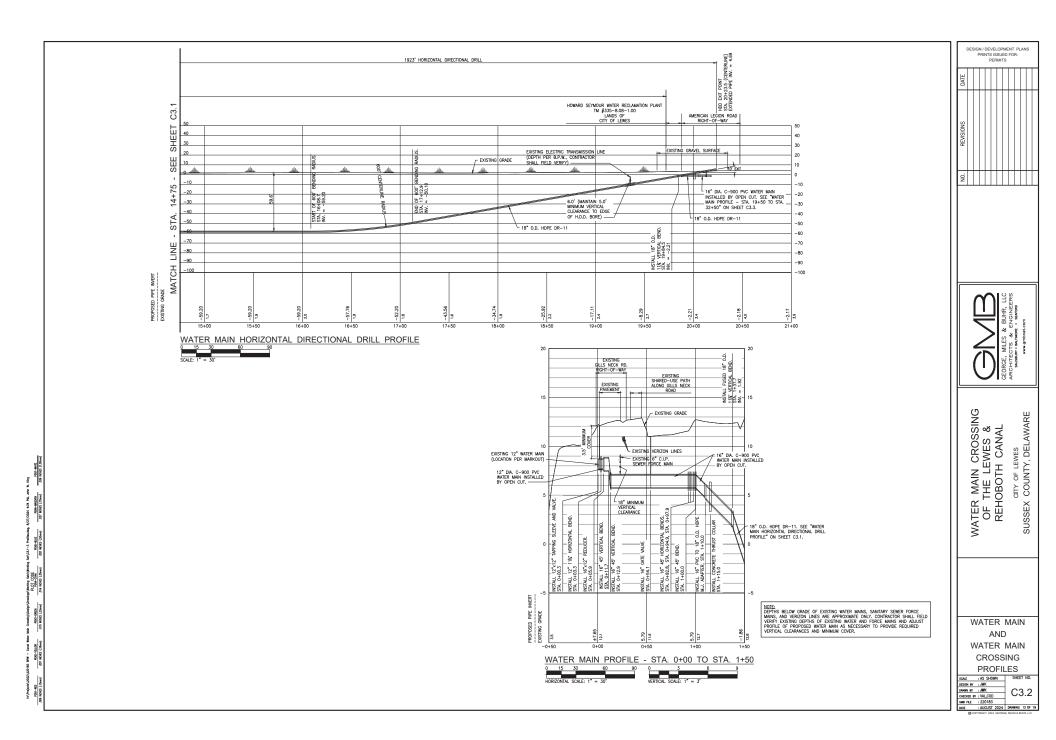
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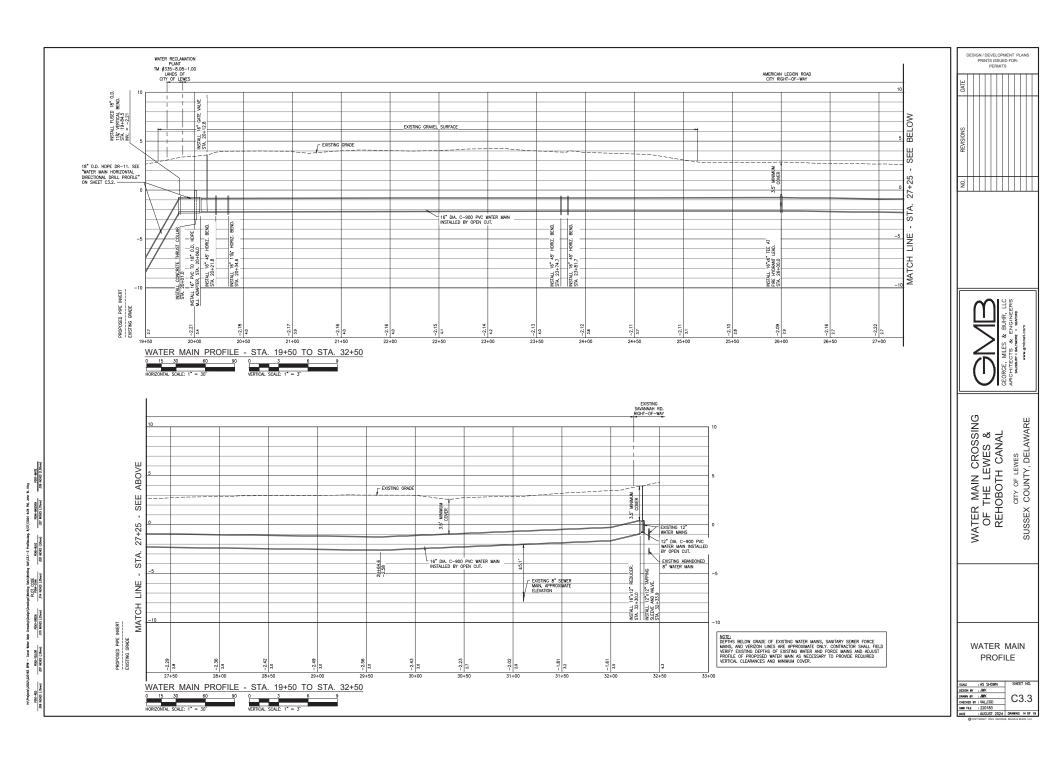
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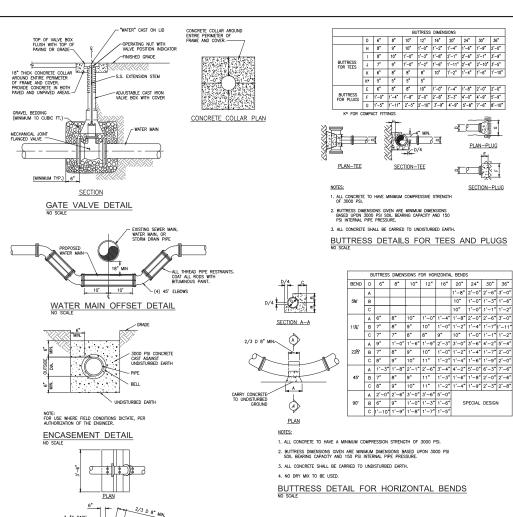








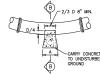




EXISTING PIPE TO REMAI Existing Pipe To Remain

SECTIONAL ELEVATION

PLUGGING ABANDONED LINES DETAIL



ELEVATION



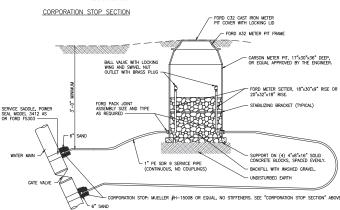
BEND	D	6"	8"	10"	12*	16"	20"	24"	30"	36"
	Α						1'-8*	2'-0"	2'-6"	3'-0*
11%	В						10"	1'-0"	1'-3"	1'-6"
	С						10"	1'-0"	1'-1"	1"-2"
	Α	6*	8*	10"	1'-0"	1'-4"	1'-8*	2'-0"	2'-6"	3'-0"
2%	В	7*	8"	9"	10*	1'-0"	1'-2*	1'-4"	1'-7"	1'-10
	C	7"	7*	8"	8*	9"	10"	1'-0"	1'-1"	1'-2"
	Α	9"	1'-0"	1'-6"	1'-9"	2'-3"	3'-0"	3'-6"	4'-2"	5'-4"
45*	В	7*	7*	8"	10*	1'-0"	1'-2*	1'-4"	1'-7*	2'-0"
	С	7"	7*	8"	8*	9"	10*	1'-0"	1'-1"	1'-2"
90"	Α	1'-3"	1'-8"	2'-1"	2'-6"	3'-4"	4'-2"	5'-0"	6'-3"	7'-6"
	В	7*	8"	9"	11*	1'-3"	1'-6*	1'-9"	2'-3"	2'-6"
	С	7*	8"	10"	11*	1'-3"	1'-6"	1'-9"	2"-3"	2'-8"

BUTTRESS DIMENSIONS FOR VERTICAL BENDS

- ALL CONCRETE TO HAVE A MINIMUM COMPRESSION STRENGTH OF 3000 PSI.
- 2. BUTTRESS DIMENSIONS GIVEN ARE MINIMUM DIMENSIONS BASED UPON 3000 PSI SOIL BEARING CAPACITY AND 150 PSI INTERNAL PIPE PRESSURE.
- 3. ALL CONCRETE SHALL BE CARRIED TO UNDISTURBED EARTH.
- 4. NO DRY MIX TO BE USED.

BUTTRESS DETAIL FOR VERTICAL BENDS

GOOSENECK 45*



- 1. PROVIDE BRASS CAP OVER 1" OUTLET AT LOCATIONS INDICATED FOR FUTURE CONNECTION
- 2. THE CONTRACTOR SHALL ADJUST CURB STOP DEPTH AS NECESSARY TO MAINTAIN MINIMUM COVER OVER SERVICE PIPE AT SIDEWALK.
- SPECIAL CARE SHALL BE TAKEN DURING BACKFILL OPERATION TO PREVENT DAMAGE TO PIPE AT CORPORATION STOP.
- ADJACENT CORPORATION STOP LOCATIONS WITHIN 24" MUST BE STAGGERED ALONG THE PIPE AXIS BY 5".
- CORPORATION STOPS SHALL BE LOCATED A MINIMUM OF 24" FROM BELL OF PIPE (START AND END OF BELL) AT JOINT PVC WATER MAIN REQUIRES SERVICE SADDLE MANUFACTURED BY POWER SEAL, MODEL 3411 OR 3412.
- 8. METER PIT, FRAME AND COVER SHALL BE SUPPLIED BY LEWES BOARD OF PUBLIC WORKS AND INSTALLED BY THE CONTRACTOR.

TYPICAL SAMPLE/LEAK TEST SERVICE PIT DETAIL NO SCALE





WATER MAIN CROSSING OF THE LEWES & REHOBOTH CANAL COUNTY, DELAWARE

SUSSEX

WATER MAIN **DETAILS**

SCALE : NONE
DESIGN BY : JWK
DRAWN BY : JWK
CHECKED BY : VAL,COD
GMB FILE : 220183 C4.1

CONCRETE ANCHORAGE-

ELEVATION

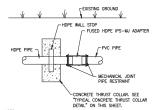
2. BUTTRESS DIMENSIONS GIVEN ARE MINIMUM DIMENSIONS BASED UPON 3000 PSI SOIL BEARING CAPACITY AND 150 PSI INTERNAL PIPE PRESSURE.

ANCHORAGE DETAIL FOR VERTICAL BENDS

1. ALL CONCRETE TO HAVE MINIMUM COMPRESSIVE STRENGTH OF 3000 PSI.

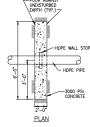
3. ALL CONCRETE SHALL BE CARRIED TO UNDISTURBED EARTH.

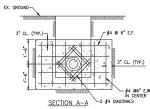




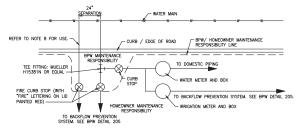
 $rac{ ext{NOTE:}}{ ext{1. ALL FITTINGS SHALL USE STAINLESS STEEL BOLTS.}}$

TRANSITION FROM HDPE TO PVC PIPE DETAIL

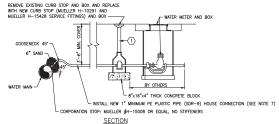




TYPICAL CONCRETE THRUST COLLAR DETAIL



SCHEMATIC PLAN



NOTES:

(1) THE CONTRACTOR SHALL ADJUST CURB STOP DEPTH AS NECESSARY TO MAINTAIN MINIMUM COVER OVER SERVICE PIPE AT SIDEWALK.

- 2. PVC WATER MAIN REQUIRES SERVICE SADDLE MANUFACTURED BY POWER SEAL, MODEL 3411 OR 3412.
- 3. CORPORATION STOP SHALL BE ORIENTED AT 45° TO HORIZONTAL AXIS OF PIPE.
- 4. SPECIAL CARE SHALL BE TAKEN DURING BACKFILL OPERATION TO PREVENT DAMAGE TO PIPE AT CORPORATION STOP.
- 5. ADJACENT CORPORATION STOP LOCATIONS WITHIN 24" MUST BE STAGGERED ALONG THE PIPE AXIS BY 5".
- 6. CORPORATION STOPS SHALL BE LOCATED A MINIMUM OF 24" FROM BELL OF PIPE (START AND END OF BELL) AT JOINT.
- SERVICE IS TO BE SIZED TO ADEQUATELY PROVIDE FIRE FLOW TO THE SPRINKLER SYSTEM. SERVICE SIZE SHALL BE PROVIDED BY A FIRE SYSTEMS ENGINEER.
- 8. WHERE AN EXISTING SERVICE IS PRESENTLY INSTALLED, OWNER SHALL INSTALL NEW FIRE SERVICE AT OWNER'S EXPENSE. ALL WORK IS SUBJECT TO INSPECTION BY LEWES BOARD OF PUBLIC WORKS. ANY DEVIATION WILL BE EVALUATED ON A CASE-BY-CASE BASIS.

TYPICAL HOUSE SERVICE CONNECTION DETAIL NO SCALE

DESIGN / DEVELOPMENT PLANS ITS ISSUED FOR: PERMITS



WATER MAIN CROSSING OF THE LEWES & REHOBOTH CANAL COUNTY, DELAWARE

SUSSEX

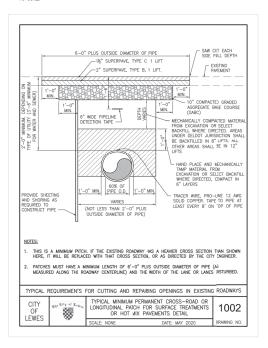
WATER MAIN **DETAILS**

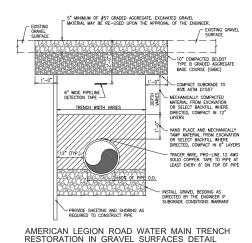
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NOTES:

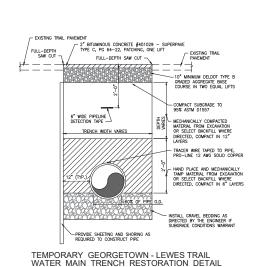
- THIS IS A MINIMUM PATCH. IF THE EXISTING ROADWAY HAS A HEAVIER CROSS SECTION THAN SHOWN HERE, IT WILL BE REPLACED WITH THAT CROSS SECTION, OR AS DIRECTED BY THE CITY ENGINEER.
- PATCHES MUST HAVE A MINIMUM LENGTH OF 6'-0" PLUS OUTSIDE DIAMETER OF PIPE (AS MEASURED ALONG THE ROADWAY CENTERLINE) AND THE WIDTH OF THE LANE OR LANES DISTURBED.

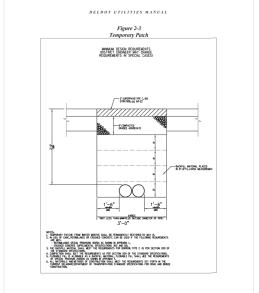
AMERICAN LEGION ROAD WATER MAIN TRENCH RESTORATION IN PAVEMENT DETAIL

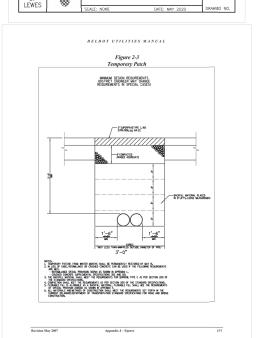




NO SCALE

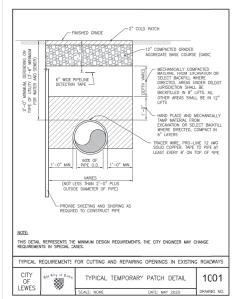






FOR TEMPORARY TRENCH PATCHES WITHIN DELDOT ROADWAYS

SEE DETAILS ON SHEET G1.1 FOR PIPE BEDDING AND PERMANENT TRENCH REPAIR WITHIN DELDOT ROADWAYS.



WATER MAIN CROSSING OF THE LEWES & REHOBOTH CANAL COUNTY, DELAWARE

DESIGN / DEVELOPMENT PLANS PERMITS

TRAIL AND ROAD RESTORATION **DETAILS**

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DESIGN BY : JWK
DRAWN BY : JWK
CHECKED BY : WAL,COD
GMB FILE : 220183 SHEET NO C4.3

PDM-MCDIA 027 NOES (Jönn)

720 NYES (50m

- 1. ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH: THE CONTROL TO COLUMENTS, THE LITEST VERSION OF THE DELAWASE AWAILA, ON UNIFORM TRAFFIC CONTROL DEVICES (DE MUTICO), CURRENT STATE OF DEVIAMED EPERATURE OF TRAFFIC PROTING ANADIADR STATEMENT OF THE DATE OF THE BUTTON, AND STANDARD SCHORTICATION DETAILS, INCLUDING ALL REMSONS AS OF THE DATE OF THE DATE OF THE STATEMENT APPRIVAL TRAFFORM THEOPERATE TRAFFIC ONTROL DEVIAES DOES NOT HAVE AN APIL, THE DEVICE SHALL BE CONTROL PRODUCTS LIST (APIL), IF A CATEGORY OF DEVICES DOES NOT HAVE AN APIL, THE DEVICE SHALL BE CONTROL WITH THE TE BUTTON TO THE STATEMENT OF THE ST
- 2. CONTRACTOR SHULL SUPPLY MESSAGE BOARDS THAT ARE TO BE FUACID TEN (10) DAYS PRIOR TO THE START OF CONSISTRACTION AND/OR TRAFFIC PATTERN CHANGES. THE MESSAGE BOARDS MUST BROWN IN A PLACE FIRE (5) DAYS AFTER WORK HAS STARTED AND/OR AFTER TRAFFIC PATTERN CHANGES HAVE BEEN IMPLEMENTED. THE NUMBER OF MESSAGE BOARDS, LOCATIONS, AND WOORDING ON THE MESSAGE BOARDS SHALL BECOMPOUNDED WITH THE DISTRICT SAFTY
- 3. IN ACCORDANCE WITH SECTION 105.1.0 OF THE STANDARD SPECIFICATIONS, THE DEPARTMENT RESERVES THE RIGHT TO SUSPEND THE CONTRACTOR'S PERMISONS, IF, IN THE OPIONAL OF THE PERMINANT'S REPERSAINATIO, THE CONTRACTOR'S OPERATIONS ARE ROTH IN COMPLANCE WITH THE DE MUTICH, THE SPECIFICATIONS OR THE PLANS OR IF THE CONTRACTOR'S OPERATIONS ARE DESIZED MUNSHES. IN ACCORDANCE WITH SECTION 8013.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING THE LOCAL 911 CENTER, LOCAL SCHOOLS AND DELDOT
 COMMUNITY RELATIONS OF ALL ROADS AND LANES TO BE CLOSED A MINIMUM OF SEVEN (7) CALENDAR DAYS BEFORE THE
 CLOSURE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT THE TRANSPORTATION MANAGEMENT CENTER (TWC) IS NOTIFIED DAILY BEFORE ROADMAY ACTIVITY IS BEING PERFORMED. IN STATE RIGHT—FOR WAY, THE CONTRACTOR SHALL IDENTIFY THE TYPE OF WORK, MAY LANG(S) OR SHOULDER(S) CLOSED, THE LENGTH OF TIME FOR WORK, WHEN THE LANE RESTRICTIONS ARE IN PLACE AND WHEN LANE RESTRICTIONS ARE LIFTED, CONTACT PERSON/PHONE NUMBER AND STATE INSPECTOR. THE TRANSPORTATION MANAGEMENT CENTER CAN BE REACHED AT (302) 659-4600.
- WHEN SIDE ROADS INTERSECT THE WORK ZONE, ADDITIONAL TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE ERECTED INCLUDING PERMANENT WARNING SIGNS (SEE NOTE 11).
- 7. ALL STORAGE OF EQUIPMENT AND MATERIAL SHALL COMPLY WITH THE DE MUTCD, SECTION 6G.21.
- ALL PAVEMENT MARKINGS THAT ARE NO LONGER IN USE AND CONFLICT WITH TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED AND COMPLETELY DRUITERED BY A METHOD APPROVED BY THE ENDINEER, PAINTING OVER THE CONFLICTING PAVEMENT MARKINGS WILL NOT BE ACCEPTED AS A METHOD OF REMOVIAL.
- THE CONTRACTOR IS RESPONSIBLE FOR THE MAINTENANCE OF EXISTING PAVEMENT WITHIN THE PROJECT LIMITS FOR THE DURATION OF THE CONTRACT OR AS DIRECTED BY THE ENGINEER.
- 10. TYPICAL APPLICATIONS PER THE DE MUTCD SHALL BE INCORPORATED TO ACHIEVE REQUIRED TEMPORARY TRAFFIC CONTROL AND SAFETY REQUIREMENTS. THIS PROJECT IS SUBJECT TO THE FOLLOWING TYPICAL APPLICATIONS UNLESS DIRECTED OTHERWISE BY THE DELIDOT DISTRICT SAFETY OFFICER:

CILIS NECK ROAD — TYPICAL APPLICATION 1: "WORK BEYOND THE SHOULDER > 10 FEET FROM THE EDGE OF THE TRANELD MAY", TYPICAL APPLICATION 3: "WORK BEYOND THE SHOULDER \le 10 FEET FROM THE EDGE OF THE TRANELED MAY", TYPICAL APPLICATION 16: "SHOULDER WORK WITH MINOR ENGROACHMENT", "TYPICAL APPLICATION 10: "LAME CLOSURE ON A TWO-LAME ROAD USING TRANSFERS".

SAVANNUM ROAD — TYPICAL APPLICATION 3: "WORK BEYOND THE SHOULDER \le 10 FEET FROM THE EDGE OF THE TRANSLED HAIT, TYPICAL APPLICATION 6: "SHOULDER WORK WITH MINOR ENCROACHMENT", TYPICAL APPLICATION 21: "LANE CLOSURE" ON THE TRANS SIDE OF AN INTERSECTION."

- LUSSING ON THE NEWS SIG. OF AN INTER-CHION.

 IN WITHIN THE WANNIEW WORK APEA, PERMANENT ADVANCE WARNING SIGNS WITH THE LEGENGS ROAD WORK AHEAD SHALL BE INSTALLED IN ADVANCE OF THE WORK APEA IN BOTH DIRECTIONS. AN EDID ROAD WORK SIGN SHALL BE LOCATED 500 FEET DOWNSTEAM FROM THE WORK APEA IN BOTH DIRECTION. ON INTERSCENCE ROADWAYS WITHIN THE PROJECT LUIST, A ROAD WORK AHEAD SIGN SHALL BE PLACED AT A DISTANCE WOT LESS THAN 500 FEET IN ADVANCE OF THE WORK APEA ON SIGN SHALL BE PLACED AT A DISTANCE WORK THAN THE PLACED THE WORK APEA ON SIGN SHALL BE REPORTED WORK OF THE WORK APEA ON SIGN SHALL BE WORKED SHAD SHALL BE REPORTED WORKED ON THE WORK THE WORK OF THE WORK APEA ON THE WORK
- 12. THE USE OF MILLINGS AND GRADED AGGREGATE BASE COURSE (GABC) IN THE TRAVEL WAY, TEMPORARY TRAVEL WAY, HIGH VOLUME ENTIMACES AND ACCESS RAMP FOR THE PURPOSE OF PROVIDING A TEMPORARY ENGAGE. FORTHCLE POTHOLOGY REPORT REPORT REPORT FOR THE FORTH AND ACCESS THE STATE OF THE PURPOSE OF PROVIDING IS CONCRETE, BITUAMOUS CONCRETE WEDGE, OR TAPER MILL, AS NOTED IN THE CONTRACT DOCUMENTS OR APPROVED BY THE EMBRISHER.

MILINES OR CARC SHALL BE USED AT THE FOLLOWING LOCATIONS WHERE ACCESS TO A BUSINESS, RESIDENCE, OR EDGE DOPO FOR HESTS TO BE JAMANIANED UNLESS OTHERWISE NOTED IN THE PLANS OF DIRECTION BY THE FORMER TO USE BITUAMOUS CONCRETE OR COLD PATCH, ALL MILLINGS AND CABC WILL BE ROLLED AND COMPACTED TO HELP PREVENT THE MATERIAL FROM UNBANGLINGS.

424 PM, Jehn W. PDM-WAZDIA NOSES (70mm)

AZO NORES (SOme)

PDCS-CREN AND NOVES (25mm)

PENG-YELLOW NOVES (Johnn)

- G. DRICEMYS

 DETAILS

 DETAILS

THE BASE COURSE MATERIAL SHALL BE PLACED AT NO GREATER THAN THE SLOPE SPECIFIED IN THE DE MUTCD, TABLE 6G-1 AND SHALL BE COMPACTED.

VERTICAL DIFFERENCES SHALL BE CORRECTED IN ACCORDANCE WITH THE DE MUTCD, TABLE 6G-1.

- 13. ACCEPTABLE MATERIALS FOR TELEGRAPY PEDESTRIAN PATHS SHALL BE AS DEFINED IN STANDARD CONSTRUCTION DETAIL. M-13. TYPE 2 TEMPORARY SIDEMALIS (PLYMOOD WALKIMAY STRUCTURES) SHALL BE CONSTRUCTED IN ACCORDANCE WITH STRAMADO CONSTRUCTOR DEFAIL M-13. TEMPORARY PEDESTRIAN PATHS SHALL BE DETECTABLE AS DEFINED IN SECTION 67.73 OF THE DE MUTCO.
- 14. CLOSURES OF TRAVEL LANES AND TURN LANES WITHIN THE PROJECT LIMITS SHALL OCCUR IN ACCORDANCE WITH THE ALLOWABLE LANE CLOSURE HOURS MATRIX SHOWN IN THE PLANS. THE PROJECT IS SUBJECT TO ADDITIONAL RESTRICTIONS IN ACCORDANCE WITH SECTION 801.3.5 OF THE STANDARD SPECIFICATIONS, ACCEPTABLE LANE CLOSURE HOURS:

TEMPORARY TRAFFIC CONTROL NOTES - AMERICAN LEGION ROAD

- THE OWNER RESERVES THE RIGHT TO STOP THE CONTRACTOR'S OPERATIONS, IF, IN THE OPINION OF THE OWNER'S REPRESENTATIVE,
 THE CONTRACTOR'S OPERATIONS ARE MOT IN COMPLIANCE WITH THE DELAMAGE MUTCD, THE SPECIFICATIONS OR THE PLANS OR IF
 THE CONTRACTOR'S OPERATIONS ARE DELEMBE UNSAFE.
- 3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING THE LOCAL 911 CENTER AND LOCAL SCHOOLS OF ALL ROADS AND LANES TO BE CLOSED A MINIMUM OF SEVEN (7) CALENDAR DAYS BEFORE THE CLOSURE.
- 4. THE CONTRACTOR IS RESPONSIBLE FOR THE MAINTENANCE OF EXISTING PAVEMENT WITHIN THE PROJECT LIMITS FOR THE DURATION OF THE CONTRACT OR AS DIRECTED BY THE ENGINEER.
- 5. TYPICAL APPLICATIONS PER THE DELAWAGE MUTICO SHALL BE INCORPORATED TO ACHEVE REQUIRED TEMPORARY TRAFFIC CONTROL AND SAFETY REQUIREMENTS. INTO PROJECT IS SUBJECT TO THE TOLORIMOS PIPOLA APPLICATIONS UNLESS DIRECTED OTHERMISE BY THE BIOGREEN PROVIA APPLICATION. "WORK BEYOND THE SHOULDER S (1) OF EXP FROM THE EDGE OF THE TRAVELED WAY," THYPICAL APPLICATION 11: "LAWE CLOSURE ON A TWO-LAWE ROAD WITH LOW TRAFFIC VOLUMES", "THYPICAL APPLICATION 21: "LAWE CLOSURE ON THE MORE SIG OF AN INTERSECTION".

TRAFFIC CONTROL SEQUENCE OF CONSTRUCTION -GILLS NECK ROAD AND SAVANNAH ROAD

- CONTRACTOR MOBILIZATION (APPROXIMATELY 5 WORKING DAYS)

 1. MOBILIZE EACH SITE (INCLUDING BUT NOT LUMITED TO):
 2.1 PLACE CONSTRUCTION TRAILERS AS NECESSARY,
 2.2 MOBILIZE AND ACQUIRE EQUIPMENT NEEDED FOR CONTRACT.
- 2. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN ON CONTRACT CONSTRUCTION
- 3 TRAIL WORK NEAR CILLS NECK ROAD PORTON (APPROXIMATELY 5 WORKING DAYS)
 3.1 USING TAIL-10 OF DELAWARE AUTICD
 3.1.1 WHEN THE BIKE TRAIL IS TO BE CLOSED A LAWE CLOSURE WITH FLAGGERS IS REQUIRED TO ROUTE BIKERS AND PEDESTA

- 4. GILLS NECK ROAD PORTION (APPROXIMATELY 5 WORKING DAYS)
 4. USING TA-10 PELAWARE WITCO 1A TIME AND INSTALL WATER MAIN.
 4. USING TA-3 AND TA-6 OF ELAWARE WITCO
 4.2.1 INSTALL UTILITIES FROM 6" FROM PAVEMENT TO RIGHT-OF-WAY LINE.
 5. USING TA-10 OF ELAWARE WITCO
 4. USING TA-10 OF ELAWARE WITCO
 4. USING TA-10 OF ELAWARE MUTCO
 4. USING TA-10 OF ELAWARE MUTCO
 4. USING TA-10 OF ELAWARE MUTCO
 5. USING TA-10 OF ELAWARE MUTCO
 5. USING TA-10 OF ELAWARE MUTCO
 5. USING TA-10 OF ELAWARE MUTCO
 6. USING TA-10 OF SELAWARE MUTCO
 6. USING TA

- 5. SAMAWHAI ROAD FORTION (JEPROLUMATELY S. WORKING DUIS)
 5. LURNO TA-10 (MARGINAL BOORD ROAD) AND TA-3, TA-6, AND TA-21 OF BELAWARE MUTCD
 5.1. OPEN CUIT HAIF THE ROAD AT A TIME AND INSTALL WATER MAIN.
 5.2 LURNO TA-10 OF DELAWARE MUTCD
 5.3 LURNO TA-10 OF DELAWARE MUTCD

**MOTE: DRIV OFFRATIONS SHOULD BE LIMITED TO THOSE THAT CAN BE INSTALLED AND CLOSED BY THE END OF EACH BURNESS DAY. REFER TO TEMPORARY PARMENTI RESTORATION DETAIL. ATTRECOMPLETION OF UTILITY MAIN INSTALLATION COMPLETE PAYING RESTORATION BY REFERRING TO PERMANENT PAYEMENT RESTORATION DETAILS FOUND IN THESE CONTRACT DRAWINGS.

6. CONTRACT CLOSEOUT (APPROX. 90 WORKING DAYS)

SAVANNAH ROAD PEDESTRIAN MAINTENANCE OF TRAFFIC

- 1. THERE ARE NO EXISTING SIDEWALKS IN THIS PROJECT AREA.
- 3. THE CONTRACTOR SHALL FOLLOW TYPICAL APPLICATIONS 28 AND 29 IN THE DELAWARE MUTCD IF APPLICABLE.
- THE FOLLOWING CONSIDERATIONS SHALL BE TAKEN INTO ACCOUNT WHEN ADDRESSING ACCESSIBLE PEDESTRIAN MAINTENANCE OF TRAFFIC:
- ALL PEDESTRIANS, INCLUDING PERSONS WITH DISABILITIES, SHALL BE PROVIDED WITH A REASONABLY SAFE, CONVENIENT AND ACCESSIBLE PATH THAT REPLICATES AS MUCH AS PRACTICABLE THE EXISTING PEDESTRIAN FACILITIES.
- . MAINTAIN ACCESS TO ALL BUSINESS AND RESIDENTS AT ALL TIMES.
- PROVIDE PEDESTRIAN ACCESS THRU OR AROUND THE WORK ZONE. IF A DETOUR IS CHOSEN THE CONTRACTOR MUST SUBMIT THE DETOUR ROUTE TO THE ENGINEER FOR REVIEW AND APPROVAL. THE DETOUR ROUTE MUST MEET OR EXCEED THE CURRENT CONDITIONS.
- WORK SHOULD BE PHASED SO THAT ALL AREAS OF AN INTERSECTION/SIDEWALK PATH ARE NOT UNDER CONSTRUCTION AT THE SAME TIME UNLESS A CLEAR PATH IS PROVIDED AND IDENTIFIED.
- SIGNS AND OTHER DEVICES MOUNTED LOWER THAN 7 FT ABOVE THE TEMPORARY PEDESTRIAN PATHWAY SHALL NOT PROJECT MORE THAN 4 IN. INTO ACCESSIBLE PEDESTRIAN ROUTE.
- A SMOOTH, CONTINUOUS HARD SURFACE SHALL BE PROVIDED THROUGHOUT THE ENTIRE LENGTH AND WIDTH OF THE PEDESTRIAN ROUTE THROUGHOUT CONSTRUCTION. THERE SHALL BE NO CURBS OF VERTICAL ELEVATION CHANGES GREATER THAN \$M IN. IN. GRADE OF TERRAIN THAT COULD CAUSE TRIPPING OR BE A BARRIBER TO WHECCHAIR USE.

GILLS NECK ROAD PEDESTRIAN MAINTENANCE OF TRAFFIC

- THERE IS THE EXISTING GEORGETOWN LEWES TRAIL IN THIS PROJECT AREA
- 2. THERE ARE NO EXISTING CROSSWALKS IN THIS PROJECT AREA
- 3. THE CONTRACTOR SHALL FOLLOW TYPICAL APPLICATIONS 28 AND 29 IN THE DELAWARE MUTCD IF APPLICABLE.
- THE FOLLOWING CONSIDERATIONS SHALL BE TAKEN INTO ACCOUNT WHEN ADDRESSING ACCESSIBLE PEDESTRIAN MAINTENANCE OF TRAFFIC:
- ALL PEDESTRIANS, INCLUDING PERSONS WITH DISABILITIES, SHALL BE PROVIDED WITH A REASONABLY SAFE, CONVENIENT AND ACCESSIBLE PATH THAT REPUICATES AS MUCH AS PRACTICABLE THE EXISTING PEDESTRIAN FACILITIES.
- MAINTAIN ACCESS TO ALL BUSINESS AND RESIDENTS AT ALL TIMES.
- PROVIDE PEDESTRIAN ACCESS THRU OR AROUND THE WORK ZONE. IF A DETOUR IS CHOSEN THE CONTRACTOR MUST SUBMIT THE DETOUR ROUTE TO THE ENGINEER FOR REVIEW AND APPROVAL. THE DETOUR ROUTE MUST MEET OR EXCEED THE QUIRENT COMPINIONS.
- WORK SHOULD BE PHASED SO THAT ALL AREAS OF AN INTERSECTION/SIDEWALK PATH ARE NOT UNDER CONSTRUCTION AT THE SAME TIME UNLESS A CLEAR PATH IS PROVIDED AND IDENTIFIED.
- TRAFFIC CONTROL DEVICES AND OTHER CONSTRUCTION MATERIALS AND FEATURES SHALL NOT INTRUDE INTO THE USABLE WIDTH OF THE SIDEWALK, TEMPORARY PATHWAY OR OTHER PEDESTRIAN FACILITY.
- SIGNS AND OTHER DEVICES MOUNTED LOWER THAN 7 FT ABOVE THE TEMPORARY PEDESTRIAN PATHWAY SHALL NOT PROJECT MORE THAN 4 IN. INTO ACCESSIBLE PEDESTRIAN ROUTE.
- A SMOOTH, CONTINUOUS HARD SURFACE SHALL BE PROVIDED THROUGHOUT THE ENTITLE LENGTH AND WIDTH OF THE PEDESTRAIN ROUTE THROUGHOUT CONSTRUCTION. THERE SHALL BE NO CURBS ON VERTICAL ELEVATION CHANGES REPETER THAN X IN. IN GRADE OR TERRAIN THAT COULD CAUSE TRIPPING OR BE A BARRIER TO WHEELCHAIR USE.

BICYCLE MAINTENANCE OF TRAFFIC NOTES

- ACCORDING TO THE STATE BICYCLE MAP FOR SUSSEX COUNTY SAVANINH FOAD HAS BEEN DESIGNATED AS A REGIONAL BICYCLE ROUTE WITH BIKEWAY, CILLS NECK ROAD HAS BEEN DESIGNATED AS A CONNECTOR BICYCLE ROUTE WITHOUT BIRCHIAY. AMERICAN LEGION ROAD, A CITY ROAD, HAS NOT BEEN DESIGNATED AS A BICYCLE ROUTE.
- AT ALL TIMES, THE CONTRACTOR SHALL PROVIDE MAXIMUM WIDTH LANES TO THE EXTENT POSSIBLE TO FACILITATE BICYCLE USAGE.
- 3. THE CONTRACTOR SHALL ADD THE BISTOLE IN LANE (MITH-I-DE) SIGN TO THE TYPICAL APPLICATION SIGNING STOKO DURNING CONSTRUCTION ACTIVITIES THAT RESTRICT LUSS OF THE EXISTING SHOULDER/BISE LANE. WHEN USED, THE SIGN SHALL HAVE A BLACK LEGEND ON A RETROREFLECTIVE FLUORESCENT GRANGE BICKGROUND.
- WHEN THE BIKE TRAIL IS TO BE CLOSED A LANE CLOSURE WITH FLAGGERS IS REQUIRED TO ROUTE BIKERS AND PEDESTRIANS AROUND THE WORK ZONE.



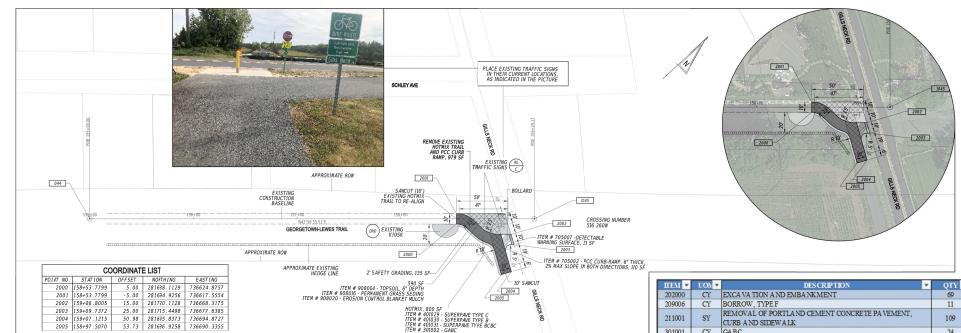


CITY OF LEWES COUNTY, DELAWARE CROSSING EWES & I CANAL WATER MAIN C OF THE LEW REHOBOTH (

SUSSEX

TRAFFIC CONTROL PLAN

SCALE : NONE
DESIGN BY : JWK
DRAWN BY : JWK
CHECKED BY : VAL,COD SHEET NO C5.1 MB FILE : 220183



CONS	XISTING)			
POINT NO.	STATION	OFFSET	NOFTHING	EASTING
1044	155+00.00	0.00	281432.5419	736380.1949
1045	159+29 17	0.00	281746 7087	736672 5784

DATUM REFERENCE:

2004

159+07 1215

2005 158+97.5070

HORIZONTAL - THIS PROJECT IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM, NORTH AMERICAN DATUM OF 1983 (NAD 83 / 2011 / EFOCH 2010.00).

50.98 281635.8373 736694.8727 53.73 281636.9258 736690.3355

VERTICAL - THIS PROJECT IS REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD &8 BASED ON MODELED GEOID 12A).

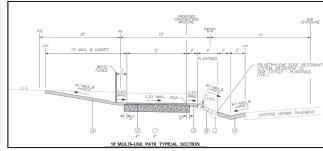
- DURING THE CONSTRUCTION PHASE OF THE PROPOSED IMPROVEMENTS, MAINTENANCE OF TRAFFIC MAY BE REQUIRED.
- LARGE UTILITY POLE RELOCATIONS ARE OUTSIDE THE SCOPE OF THIS PROJECT.
- PLAN WAS DEVELOPED ON AERIAL PHOTOGRAPHY WITHOUT TOPOGRAPHIC SURVEY.

ASSUMPTIONS AND LIMITING CONDITIONS

- NO TOPOGRAPHIC OR BOUNDARY SURVEY WAS PERFORMED; ALL ROADWAY DATA AND UTILITY LOCATIONS WERE DERIVED FROM AERIAL PHOTOGRAPHY. FURTHER VERIFICATION OF UTILITY LOCATIONS ARE NEEDED DURING DESIGN PHASE.
- EXISTING RIGHT-OF-WAY LINES WERE BASED ON AERIAL IMAGERY AND DELDOT'S GIS DATA. FURTHER RIGHT-OF-WAY VERIFICATION IS NEEDED DURING DESIGN PHASE.
- THE PROJECT IS ANTICIPATED TO COMPLY WITH THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS (DSSR) BASED ON STANDARD PLAN CRITERIA FOR SIDEWALK, TRAIL OR OTHER LINEAR IMPERVIOUS SURFACES.

- WIEN NECESSARY, CONSTRUCT INLET SEDIMENT CONTROL, CURB INLET AND FOLLOW THE STANDARD CHASTAUCTION DETAILS OR MANUFACTURES RECOMMENDATIONS. ALL COSTS INCIDENTAL TO THE MOSDOS LINET SEDIMENT CONTROL, CURB INLET.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO USE THE PROPER CONSTRUCTION EQUIPMENT AS MCCESSARY TO MEET ACCESSIBILITY REQUIREMENT AS SET FORM IN DICLOT'S PEDESTRIAN ACCESSIBILITY STANDARDS FOR FACILITIES IN THE PUBLIC RIGHT-OF-MAY IT INFORMSEN PHYSICAL LIMITATIONS ARE ENCONTREED DURING CONSTRUCTION. THE CONTRACTOR SHALL STEP WORK AND NOTIFY THE ENGINEER, THE CONTRACTOR SHALL RESUME WORK ACTIVITIES AFTER THE ENRINEER DETERMINES IF THE PLAN NEEDS TO BE WOOTIED OR REVISED.
- REIER TO STANDARD PLAN SHEETS FOR ADDITIONAL NOTES & DETAILS OF TYPE 1 TO TYPE 5 PEDESTRIAN CONNECTION.
- CONTICTION.

 THE CONTRACTOR SHALL STAKE ALL GRADES TO EVSURE POSITIVE DRAINAGE PRIOR TO CONSTRUCTION.
 ALL GRADES WUST BE APPROVED BY THE ENGINEER IN THE FIELD PRIOR TO CONSTRUCTION. ALL WORK
 REGUIRED FOR CACULATING AND STAKING OF GRADES SHALL BE PAID UNDER ITEM 763621
 CONSTRUCTION ENGINEERING, REHABILITATION.
- CONSTRUCTION ENGINEERING, REPRODUCTIONS OF THE NEW CONSTRUCTION, THE CONTRACTOR IN AREAS MIRES WHITE EDGE LINES WILL BE REMUED BECAUSE OF THE NEW CONSTRUCTION, THE CONTRACTOR SHALL REPLACE THE WHITE LINES IN KIND IN ORDER TO MACH EXISTING CONDITIONS. THE CONTRACTOR WAS TO CONSTRUCTION.
- TRAL TO MATCH EXISTING GRADE OR AS DIRECTE? IN THE FIELD BY THE ENGINEER. THE PROPOSED SOLWALK CROSS SLOPE SHALL NOT EXCEED 2% PPA ADA REGULATIONS. THE SIDENALK SHALL BE LAID OUT FOR APPROVAL BY THE ENGINEER PRIOR TO INSTALLATION. ALL COSTS ARE INCIDENTAL TO THE PROJECT.
- RAMPS: 2% MAX. (50:1) CROSS SLOPE & 8.33% MAX. (12:1) RUNNING SLOPE ALLTRUNCATED DOMES ARE TO BE 2' WIDE WITH A 2% MAX. (50:1) CROSS SLOPE



3 4 5 5 7 7 9	COA	DESCRIETION .	Q11
202000	CY	EXCA VA TION A ND EMBA NKMENT	69
209006	CY	BORROW, TYPEF	11
211001	SY	REMOVAL OF PORTLAND CEMENT CONCRETE PAVEMENT, CURB AND SIDEWALK	109
301001	CY	GABC	24
401005	TON	SUPERPAVE TYPE C, PG 64-22 (CA RBONA TE STONE)	22
705002	SF	PCC SIDEWALK, 6"	110
705007	SF	DETECTABLE WARNING SURFACE	21
720556	EACH	BOLLARD	1
762000	LF	SA W CUTTING, BITUMINOUS CONCRETE	20
763621	HOUR	CONSTRUCTION ENGINEERING, REHABILITATION	24
801000	LS	MAINTENANCE OF TRAFFIC	1
819018	EACH	INSTALLATION OR REMOVAL OF TRAFFIC SIGN(S) ON SINGLE SIGN POST	4
908004	SY	TOPSOIL, 6" DEPTH	81
908016	SY	PERMANENT GRASS SEEDING, SUBDIVISION	81
908020	SY	EROSION CONTROL BLANKET MULCH	81

LEGEND

- 1) ITEM 301001 GABC
- (2) ITEM 401005 SUPERPAVE TYPE C., PG64-22 (CARBONATE STONE)
- 3 ITEM 209006 BORROW, TYPE F (IF REQUIRED TO FILL VOIDS)
- 4 ITEM 908004 TOPSOIL, 6" DEPTH ITEM 908020 - PERMANENT GRASS SEEDING, SUNDIVISION
- ITEM 908020 EROSION CONTROL BLANKET MULCH 8 ITEM - PLANTING WITH WOOD MULCH, WHEN APPLICABLE

REVISIONS									V	
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CONTRACT	BRIDGE NO.	N/A	RF-ALIGNMEN
TXXXXXXXXX		14//1	KE-ALIGINIVIEN
IMMARKAMAK	DESIGNED BY:	ABEBAW BERIHUN	AT GILLS
COUNTY			AI OILLO
SUSSEX	CHECKED BY:	THOMAS FELICE	JANUAI

NT OF G-L TRAIL PLANNING S NECK RD SHEET NO. ARY 2024 01